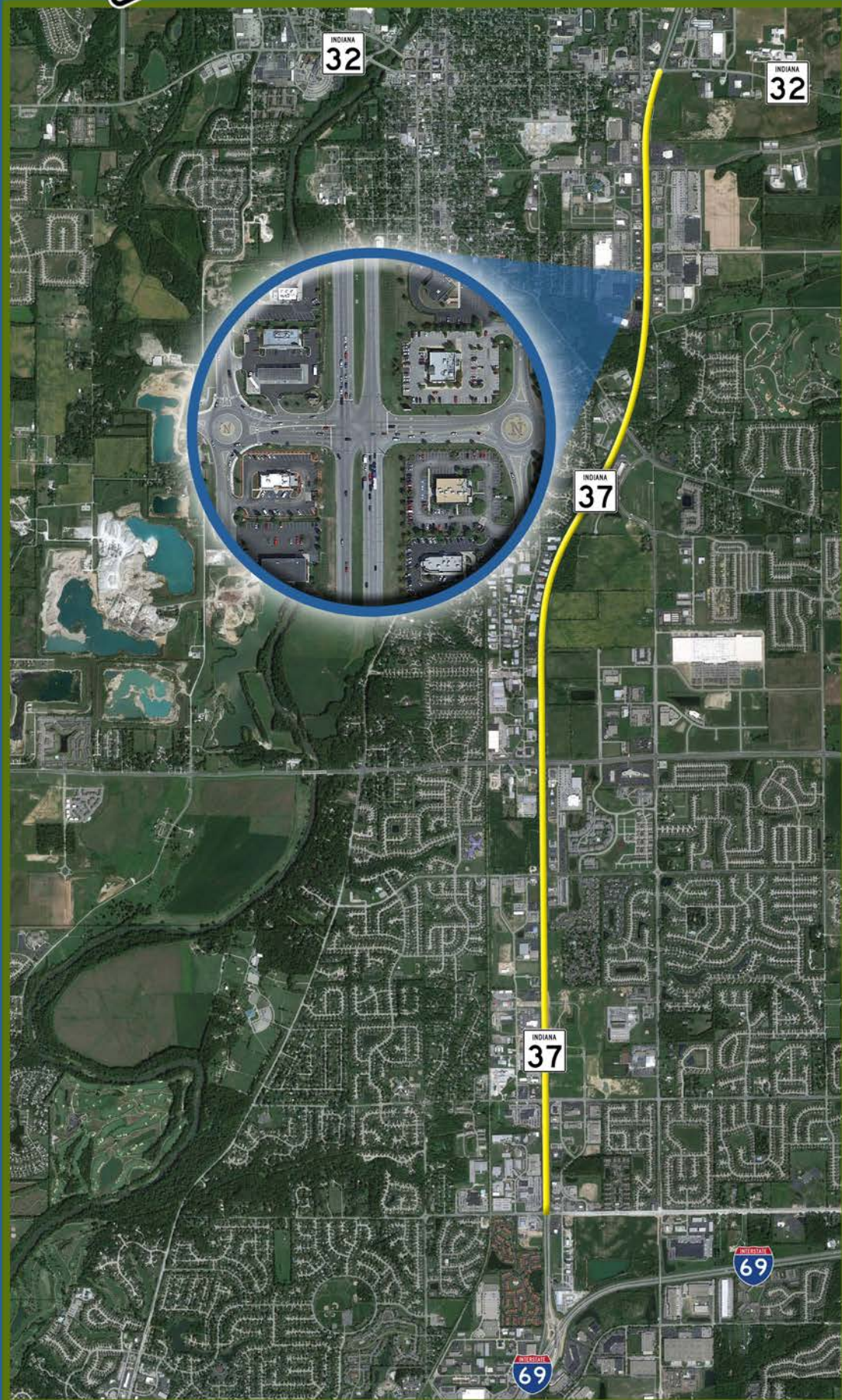




SR 37 MOBILITY STUDY



SR 37 AND TOWN & COUNTRY BLVD.

Presented to:



Presented by:





SR 37 Mobility Study (Study) Town and Country Boulevard at SR 37

Description of Proposed Project

I. GENERAL

The Indiana Department of Transportation, the Indianapolis Metropolitan Planning Organization, Hamilton County, Town of Fishers, and City of Noblesville have identified the need to significantly improve the SR 37 corridor from 126th Street to SR 32 / SR 38. The Study area also extends along 146th Street from Allisonville Road to Cumberland Road. The Study was funded 80% by the Federal Highway Administration through the MPO with the remainder provided by Hamilton County.

II. PURPOSE

The purpose of the Study was to evaluate whether grade separation of the existing intersections would improve the traffic capacity, efficiency, and safety for the project corridors without the need to add additional travel lanes along this segment of the SR 37 corridor. This includes the basic concept of reconstructing each of the existing and anticipated signalized intersections through this segment of SR 37 to interchanges; thus eliminating the need for added travel lanes along the corridor. If this was shown to be an improvement, then the Study was to further identify a preferred design solution for future improvements along the SR 37 corridor and to identify potential environmental concerns that may be present, and to establish a reliable budget to construct these improvements.

The preferred design solution was defined to a level which will allow officials with the INDOT, MPO, Hamilton County, Town of Fishers, and the City of Noblesville to begin making necessary amendments to their requisite Planning Documents.

III. EXISTING FACILITY

The subject corridor is located in south central Hamilton County in Delaware and Noblesville Townships, and in the Town of Fishers and City of Noblesville. SR 37 runs south to north through Hamilton County; including the Study area. Additionally, SR 37 is intersected by I-69 immediately south of the Study area. SR 37 is designated as a state highway in central Indiana. Near the Study area SR 37 begins at I-69 and proceeds in a northerly direction before terminating in the City of Marion, Grant County. The items identified in bold below show the existing roadway system at SR 37 and Town and Country Boulevard:

Table 1 – Existing Roadway System				
Facility	Traffic Control	Travel Lanes	Functional Classification	Speed Limit (MPH)
SR 37	-	4	Expressway	55
Town and Country Boulevard	Signal	2	Collector	35

The following paragraphs give additional details for existing Town and Country Boulevard within the Study area:

Town and Country Boulevard

Town and Country Boulevard crosses SR 37 at a right angle to form a four way at-grade intersection. SR 37 is classified as an Expressway through the limits of this intersection and has limited access right of way. Through the limits of this intersection, SR 37 is a four lane roadway with two 12-foot travel lanes, a four foot paved inside shoulder and ten foot paved outside shoulder. The northbound and southbound travel lanes are separated by a 50 foot open grass median. The existing pavement is full depth HMA and is in good condition. Next to all SR 37 turn lanes at the intersection, the shoulders are four feet wide.

Town and Country Boulevard is classified as a Collector through the limits of this intersection and is not access controlled. This road runs east to west and connects Clover Road on the west side of SR 37 to Mercantile Road on the east side of SR 37. Clover Road and Mercantile Boulevard run north to north and are used as frontage roads to access numerous retail businesses on each side of SR 37. These two roads intersect Town and Country Boulevard with roundabouts on each side of SR 37. The Clover Road roundabout is approximately 250 feet west of SR 37 and the Mercantile Boulevard roundabout is approximately 300 feet east of SR 37. Town and Country Boulevard is bordered by curb and gutter on the outside on each side of SR 37. On the west side of SR 37, Town and Country Boulevard has a four foot raised center median with light poles and small shrubs in the median. The existing pavement is full depth HMA and is in good condition. There is a six foot sidewalk adjacent to the curb in the two east quadrants of the Clover Road roundabout; however the sidewalk ends at the pedestrian crossing just east of Clover Road and the sidewalk does not extend along Town and Country Boulevard.

The intersection of SR 37 and Town and Country Boulevard is a signalized intersection, operating as an 8 phase signal with protected left turns in each direction. Approaching the intersection, SR 37 has one left turn lane, two through lanes, and one right turn lane on each approach. Town and Country has one left turn lane, two through lanes, and one right turn lane on the eastbound approach; and two left turn lanes, one through lane, and one right turn lane on the westbound approach. The westbound approach to Clover Road has one shared left/through lane and one right turn lane. The east bound approach to Mercantile Boulevard has one left turn lane and a shared right/through lane.

The intersection is bordered by businesses in each quadrants, which are located in close proximity to the intersection with little setback. For a listing of each business adjacent to the intersection, see the aerial displays.

IV. EVALUATED BUILD ALTERNATIVES

The Study evaluated two primary build alternatives: upgrading the existing SR 37 corridor with either teardrop roundabout interchanges (Alternative 1) or tight diamond interchanges (Alternative 2). Both alternatives will significantly improve traffic operations at the Study intersections.

V. TRAFFIC OPERATION ANALYSIS

Table 2 shows the results of the Capacity Analysis for the proposed improvements at SR 37 and Town and Country Boulevard for the study year of 2036. A teardrop roundabout interchange is proposed, with a 4-lane bridge crossing SR 37.

Table 2 – Alternative 1 (2036) Capacity Analysis												
Intersection	Traffic Control	Peak	West Leg		East Leg		South Leg		North Leg		Overall	
			LOS	Delay (sec)	LOS	Delay (sec)	LOS	Delay (sec)	LOS	Delay (sec)	LOS	Delay (sec)
SR 37 NB Ramps and Town & Country Blvd	Roundabout	AM	A	1.8	A	1.8	A	1.8	--	--	A	1.8
		PM	A	2.4	A	3.0	A	4.2	--	--	A	3.2
SR 37 SB Ramps and Town & Country Blvd	Roundabout	AM	A	1.8	A	1.8	--	--	A	2.4	A	2.0
		PM	A	8.4	A	2.4	--	--	A	6.0	A	5.8

Please see the Traffic Operation Analysis (binder labeled Traffic Operation Analysis) to review the Study area results in their entirety.

VI. GEOTECHNICAL EVALUATION

The corridor is located in a glaciated area. With the exception of the area near Stony Creek, the alignment is within a typical Central Indiana profile that consists of softer and moderate-plasticity clays overlying hard and low-plasticity clays, and bedrock is over 100 feet deep. The harder clays are usually within 20 feet of the surface. In addition, frequent seams and layers of granular soils can be encountered. This profile typically includes seasonal perched groundwater conditions within a few feet of the surface. From a design and construction perspective, CBR values are commonly in the range of 3 to 4, and subsurface drainage is typically required for pavement and below-grade structures (e.g., cut walls). Because of the perched groundwater and the clayey soils, improvement of the subgrade for support of pavement and construction activities is usually required, particularly in areas of cut. Support of bridges on driven piling and/or spread foundations is anticipated to be viable. In addition, support of MSE walls in these conditions typically includes preparation of the subgrade for the leveling pad and structure fill.

Cut walls over about 12 feet in height are anticipated to required tie-backs in order to control deflections, and the length of tie-backs is typically in the range of 25 to 50 ft.

In the area of Stony Creek, the soil profile is anticipated to be glacial outwash and/or alluvium (i.e., primarily granular soil), and bedrock could be within 50 to 100 feet of the surface. The issues related to shallow perched groundwater are usually not of concern because of the well-drained profile. However, if the piezometric groundwater level is relatively shallow, a large influx of groundwater can require temporary and permanent dewatering in areas of cut. An additional construction cost of \$500,000 is anticipated for each the Greenfield Avenue and Town and Country intersections to mitigate this condition.

A Geotechnical Evaluation will be required to evaluate the subsurface conditions and to provide the necessary information for a pavement design. This will include soil borings and a formal Geotechnical Report with recommendations that will be approved by INDOT.

VII. ENVIRONMENTAL INVESTIGATION

Improvements to this intersection will require the completion of an environmental document to qualify for federal funding. A Categorical Exclusion as falling within the guidelines of the National List of Categorical Exclusions will be required for this project. The Categorical Exclusion will need to be prepared in a manner consistent with the latest version of the "Indiana Categorical Exclusion Manual". The paragraphs below highlight the key environmental issues associated with the proposed project.

Wetland and Stream Impacts

The National Wetland Inventory (NWI) Map shows no wetlands or streams within the project limits. A "Waters of the U.S." report (wetland determination/delineation) will be required to verify the NWI map.

Historic and Cultural Resources

Archeological: The proposed project will result in the acquisition of undisturbed right-of-way. As a result, an Archaeological Records Review and Phase Ia Archaeological Survey will be required to identify potentially significant cultural resources within the preferred alignment.

Historical: The land use in close proximity and within the project area consists of recreational and residential properties. The *Hamilton County Interim Report* shows no historic properties within the probable Area of Potential Effects. However, properties may have become 50 years of age since the publication of the interim report.

At a minimum, this project will require the completion of the following Section 106 documents: Phase Ia Archaeological Survey, Historic Properties Report and a Section 106 Findings and Determinations (36 CFR 800.11).

Hazardous Materials

A search of the red flag indicators revealed underground storage tank and leaking underground storage tank sites in the project vicinity. As a result, further investigation will be required to determine if the project would be impacted by hazardous materials.

Regulatory Permits

IDEM Rule 5 Permit: Since the project will disturb greater than one acre, Rule 5 administered through the Indiana Department of Environmental Management will apply to this project. The designer shall coordinate all erosion and sediment control measures with the Hamilton County Soil and Water Conservation District.

VIII. DRAINAGE

The existing drainage on Town and Country Boulevard is conveyed by sheet draining the pavement to the outside curb and gutters. Curb and gutter turnouts are utilized to outlet the storm water into the ditches along SR 37. On mainline SR 37, the existing drainage is conveyed by an open grass median and outside ditches flowing south to Stony Creek about 1440 feet south of the intersection.

The proposed drainage on Town and Country Boulevard will utilize an enclosed storm sewer system consisting of curb and gutter inlets spaced appropriately which will connect to manholes. These manholes will then convey the water to an outside ditch along SR 37 where there is positive drainage from the ditch to Stony Creek approximately 1440 feet south of the intersection. The drainage on SR 37 will be handled similarly. Inlets will be spaced along both sides of the median barrier as well as on the outsides against the walls. The inlets that are within the limits of the depressed profile will be conveyed by manholes to a lift station.

The lift station will be located in the southwest quadrant between the ramp and the wall on SR 37. A 20-inch forcemain is proposed between the wet well and the discharge location approximately 1440 feet south of the intersection out to Stony Creek. The proposed lift station will include two centrifugal submersible pumps for stormwater runoff installed within a precast concrete wet-well. An additional precast concrete valve vault will be installed adjacent to the wet well. An above grade control panel will be mounted on a pedestal at a discrete location near the lift station and a generator will be included for emergency backup power. The lift station will have a firm pumping capacity (one pump out of service) of 6,400 gpm.

IX. UTILITY COORDINATION

The following paragraphs give details pertaining to the presence of utilities at Town and Country Boulevard and SR 37. This is followed by a discussion of potential impacts resulting from the project.

Existing Facilities

UNITED conducted a site visit to identify existing utilities. Based on observations of above ground facilities (ie, manholes, valve boxes, pedestals, utility markers), we identified likely underground facilities. If more accurate information is required, "Holey Moley" or the individual utilities can be contacted.

There is no evidence of any utilities along Town and Country Boulevard. It appears that facilities for the development on each side of SR 37 are internal and do not cross SR 37.

Street Lighting: Decorative street lights are located around the eastern roundabout.

Impacts

The preferred alternative has Town and Country Boulevard going over SR 37 and connecting to the existing roundabouts on the west and east side. If there are existing utilities along Town and Country Boulevard, minor relocation work is expected based on relatively small change in profile grade.

All existing utilities appear to be in the existing right-of-way and are not eligible for reimbursement of relocation costs.

X. PROPOSED INTERSECTION FACILITY

SR 37

Existing SR 37 is a four lane expressway with four 12-foot travel lanes, four foot inside shoulders, and ten foot outside shoulders. The northbound and southbound travel lanes are separated by a 50 foot open grass median (inside travel lane to inside travel lane). The existing right-of-way along SR 37 varies from mostly 85 feet to 95 feet from centerline on both sides. Many businesses line each side of the SR 37 right-of-way throughout the Study limits. The interchanges proposed in this Study require auxiliary lanes, ramp junctions, and ramp lanes adjacent to SR 37 travel lanes approaching each interchange from each side. Additional right-of-way will be required in many locations adjacent to ramp lanes and junctions. In an effort to minimize the amount of right-of-way required and the impacts to existing businesses, it is proposed that the SR 37 median be enclosed with a center median barrier and the SR 37 travel lanes be shifted in to narrow the width of the roadway through the interchange limits.

A 14.5 foot median is proposed, consisting of six foot inside shoulders and a 2.5 foot median barrier wall. Six foot is the desirable inside shoulder width required using Table 53-6 from the Indiana Design Manual (IDM). See the typical cross sections in this Study for full roadway dimensions. If any, one isolated interchange is constructed, the SR 37 travel lanes would shift back out on the north and south sides of the interchange to match the existing travel lanes and median width. As consecutive interchanges are constructed, it will not be feasible to shift lanes out to the existing median width and back in between most interchanges. If all interchanges were built concurrently, the median would remain enclosed from the south side of 126th Street to the north side of 146th Street, and from the south side of Town & Country Boulevard to the north side of SR 32 / SR 38. As there is sufficient distance between 146th Street and Greenfield Avenue, the travel lanes north of 146th Street could shift out the existing median width even if the 146th Street and Greenfield Avenue interchanges were constructed at the same time or consecutively. Furthermore, because of the layout and surrounding parcels at Greenfield Avenue, it is feasible to maintain the existing open median width through this location even when the proposed interchange is constructed. Where this is cost prohibitive at other locations due to right-of-way and business impacts associated with the wider roadway, it is economically feasible at the Greenfield Avenue Interchange. The travel lanes would shift back into an

enclosed median south of Town and Country Boulevard and remain enclosed to north of SR 32 / SR 38, where the lanes would shift back out to meet the existing pavement.

This Study focuses on the interchanges; however the treatment of SR 37 proper, between the interchanges will be affected by each interchange's traffic and proximity to each other. The geometrics developed for this Study are unique to each area between interchanges according to the findings of the Traffic Operations Analysis (TOA) conducted as part of this Study. In each segment between interchanges, in both directions, there will be an entrance ramp junction from one interchange followed by an exit ramp junction to the next interchange. This creates weaving areas between the interchanges, which were analyzed in the TOA. Some weaving areas were acceptable and are recommended. Other weaving areas are not acceptable and have been removed by interconnecting consecutive interchanges with collector distributor lanes. See the TOA for the discussion and results of the weaving analysis conducted between interchanges. Below is a summary of the proposed configuration of SR 37 near Town and Country Boulevard:

Between Greenfield Avenue and Town and Country Boulevard

Both northbound and southbound weaving segments are acceptable. The northbound entrance ramp from Greenfield Avenue and the northbound exit ramp to Town and Country Boulevard will be conventional entrance and exit ramps. The southbound entrance ramp from Town and Country Boulevard and the southbound exit ramp to Greenfield Avenue will be conventional entrance and exit ramps. There will be a continuous auxiliary lane between interchanges in both directions.

Between these two interchanges, the travel lanes will need to shift inward to transition from the existing open grass median carried through the Greenfield Avenue interchange, to the proposed enclosed median prior to the Town and Country Boulevard interchange.

Between Town and Country Boulevard and Pleasant Street

The northbound weaving segment is acceptable; however the southbound weaving segment fails. The northbound entrance ramp from Town and Country Boulevard and the northbound exit ramp to Pleasant Street will be conventional entrance and exit ramps. There will be a continuous auxiliary lane between the interchanges in the northbound direction. In the southbound direction, a continuous collector-distributor (CD) lane will be used to interconnect the interchanges. Only the southbound exit to Town and Country Boulevard is proposed, exiting to the CD. Traffic wishing to enter southbound SR 37 from Pleasant Street will travel through the CD and enter south of Town and Country Boulevard.

Town and Country Boulevard

The preferred alternate for this intersection is to construct a "teardrop" roundabout interchange on Town and Country Boulevard consisting of two closely spaced roundabouts on either side of SR 37, which are tied together through the middle to function as one unit. Town and Country Boulevard will overpass SR 37. SR 37 will be free-flow through this interchange and traffic

traveling through on Town and Country Boulevard will drive through the roundabouts with a yield condition on the roundabout approach.

The layout of the ramps will closely resemble a tight diamond interchange with directional entrance and exit ramps in each quadrant. Beyond the back of the gore area, all four ramps will remain directly adjacent to SR 37 maintaining an approximate 22 foot offset from outside edge of the SR 37 travel lane to the inside edge of the ramp lane(s). This offset allows for the minimum outside mainline shoulder, minimum inside ramp shoulder and the wall in between the mainline and the ramps. This wall is necessary to maintain the elevation difference between the mainline and the ramps as they approach Town and Country Boulevard. Exterior walls will also be necessary in each quadrant to minimize impacts to businesses in these quadrants (See aerial sheets for estimated wall limits).

Town and Country Boulevard will have two lanes in each direction through the east/west portion of the roundabouts. On both approaches there will be one shared left/through lane, and one shared through/right lane. The northbound exit ramp will exit as one lane and develop into two lanes at the roundabout approach, consisting of one shared left/through lane and one right turn lane. The southbound exit ramp will exit as one lane and develop into two lanes at the roundabout approach, consisting of one shared left/through lane and one shared through/right turn lane. The northbound entrance ramp will both be a one lane entrance. The southbound entrance ramp will be a two lane entrance to SR 37. For a diagram of the proposed lane configuration see the Traffic Operations Analysis (binder labeled Traffic Operation Analysis).

One current drive accesses off Town and Country Boulevard will need to be removed due to the close proximity to the interchange and the vertical difference of proposed Town and Country Boulevard in the area of the drive. This drive is a right-in only access to the BP gas station in the northwest quadrant. This business will maintain access to Town and Country Boulevard by way of an existing drive on Clover Road.

Because of the length necessary to develop entrance and exit ramp junctions, and gain vertical separation between the SR 37 and the Town and Country Boulevard, the south side of the interchange is expected to extend across the existing SR 37 bridge over Stony Creek. This will require widening of this existing bridge to accommodate the width necessary for the tapers and ramp auxiliary lane development.

XI. PROPOSED BRIDGE FACILITY

The bridge will be designed to meet or exceed the current "AASHTO LRFD Bridge Design Specifications" as supplemented by INDOT design standards. The minimum vertical clearance for roadways crossing over SR 37 is 16'-6".

The proposed bridge over SR 37 at Town and Country Boulevard is anticipated to be a two span, 118 foot long, prestressed reinforced concrete I beam structure built with no skew to the roadway. The bridge will be a four lane roundabout facility with a clear roadway width of 202'-10" and an out to out coping of 206'-2". The bridge will be designed to span the four lane SR 37 divided highway with the interior pier placed in the median of SR 37. It is anticipated that the

proposed structure will be constructed with integral end bents on piles and a concrete interior wall pier on piles. The structure will also have reinforced concrete approach slabs to provide a smooth transition from the approach roadway to the bridge and to protect the ends of the bridge from settlement and erosion. The proposed bridge will include common height concrete bridge rail with transitions, approach guardrail and end treatments to meet current minimum standards.

XII. MAINTENANCE OF TRAFFIC

The following is a logical basic MOT plan for the construction of the Town and Country Boulevard interchange:

Phase 1 – The southbound SR 37 travel lanes will be widened to the inside with temporary widening. Temporary cross-overs will be constructed in the median to the north and south of the interchange.

Phase 2 – All SR 37 traffic will run on the southbound side with two travel lanes in each direction. The southbound travel lanes will be shifted west to run on the existing outside shoulder. The northbound traffic will be switched over to the southbound side to run on the temporary widening constructed in phase 1.

The northbound half of mainline SR 37 will be constructed. A temporary cut wall will be constructed “top down” between the existing southbound lanes and the proposed northbound lanes through the interchange area where SR 37 will be depressed.

The northbound exit and entrance ramps will be constructed up to the proposed roundabout. A temporary connection will be constructed across the proposed roundabout area connecting the top of the northbound exit ramp and the top of the northbound entrance ramp.

The east end bent for the proposed bridge will also be constructed in this phase.

The east segment of Town and Country Boulevard will be closed, with no access to SR 37. The east segment of Town and Country Boulevard and roundabout approaches will be constructed.

The west segment of the Town and Country Boulevard will maintain access to SR 37. This could be set up as right-in/right-out access to and from Town and Country Boulevard with SR 37 traffic remaining free-flow through the intersection. Alternatively, a temporary signal could be utilized to allow the west Town and Country Boulevard protected access to and from both directions of SR 37.

Phase 3 – All SR 37 traffic will run on the proposed northbound lanes and shoulders constructed in phase 2, with two lanes in each direction. The southbound lanes will be switched over to the northbound side to run on the proposed northbound lanes constructed in phase 2. The northbound lanes will run up the proposed northbound exit ramp, across the temporary connection, and back down the proposed northbound entrance ramp all constructed in phase 2.

The southbound half of mainline SR 37 will be constructed, as well as the west segment of Town and Country Boulevard and the west roundabout. Both sides of Town and Country Boulevard will have no access to or from SR 37 in this phase. However, temporary connections

could be constructed on the east side between the portion of the east Town and Country Boulevard segment constructed in phase 2 and the northbound SR 37 travel lanes. If desired, this could be done to keep access to and from northbound SR 37 and the east side of Town and Country Boulevard in this phase.

XIII. LAND ACQUISITION

Approximately 19 parcels would be impacted by the construction of the teardrop roundabout interchange at the intersection of SR 37 and Town and Country Boulevard. Total permanent right of way acquisition required for construction of these improvements would be approximately 2.0 acres.

Because the project would likely utilize federal aid, future land acquisition would need to adhere to the *Uniform Relocation Assistance and Real Property Acquisition Policies for Federal and Federally Assisted Programs Act*. This process includes title research, right-of-way engineering, appraisal problem analysis (APA), an appraisal, a review appraisal and negotiations/buying with the property owner.

All existing right-of-way would be verified during the land acquisition process, which may reveal the need for additional parcels. If recorded documents do not exist, it may be necessary to reacquire portions of the apparent existing right-of-way, which could also increase the anticipated number of parcels and costs affiliated with those additional parcels.

XIV. PROJECT PRIORITIES

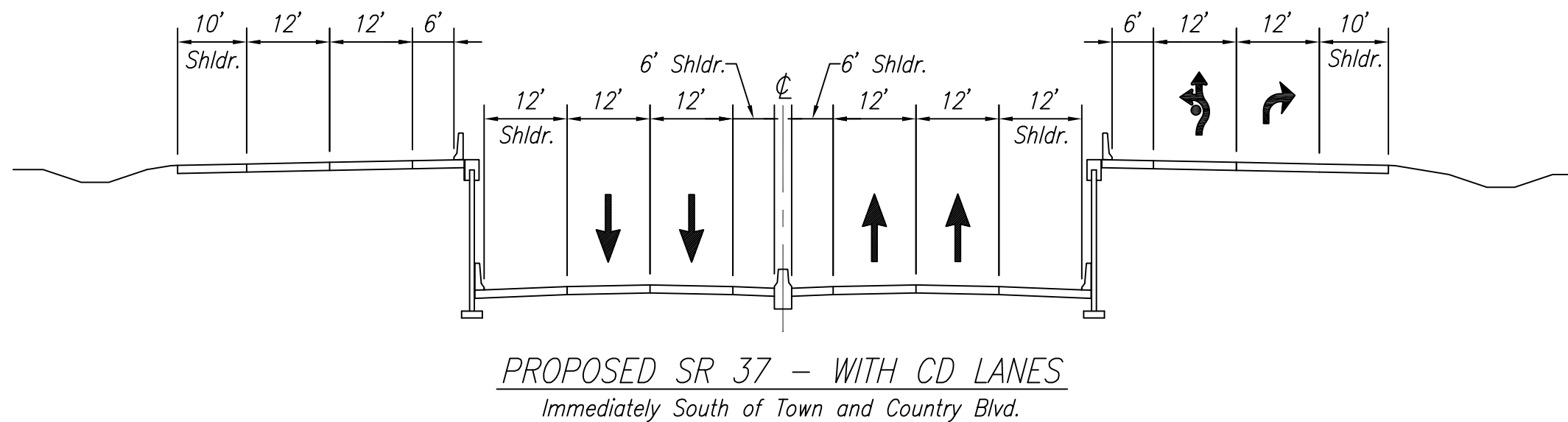
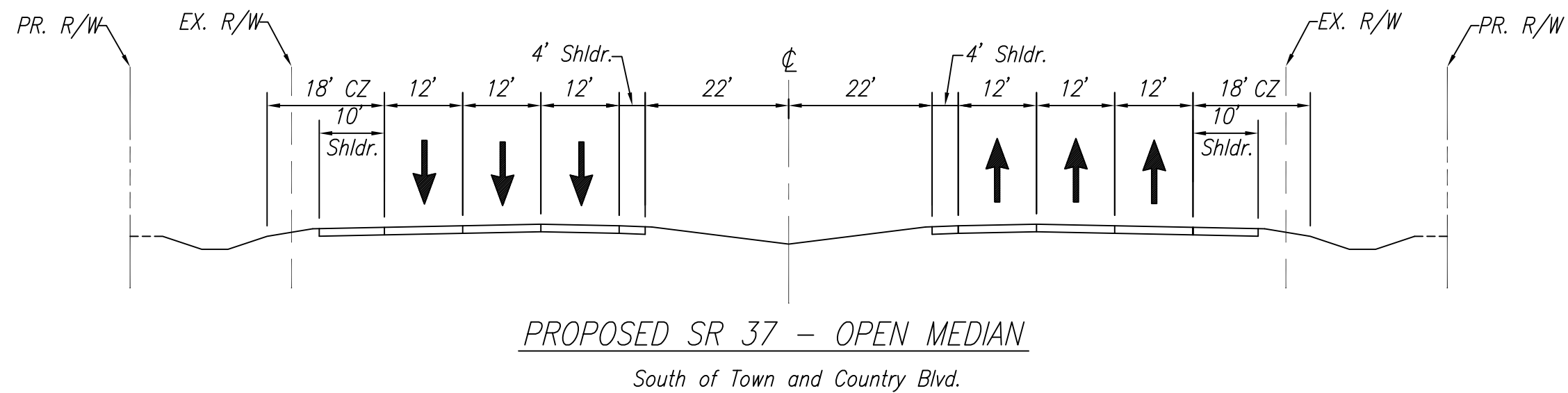
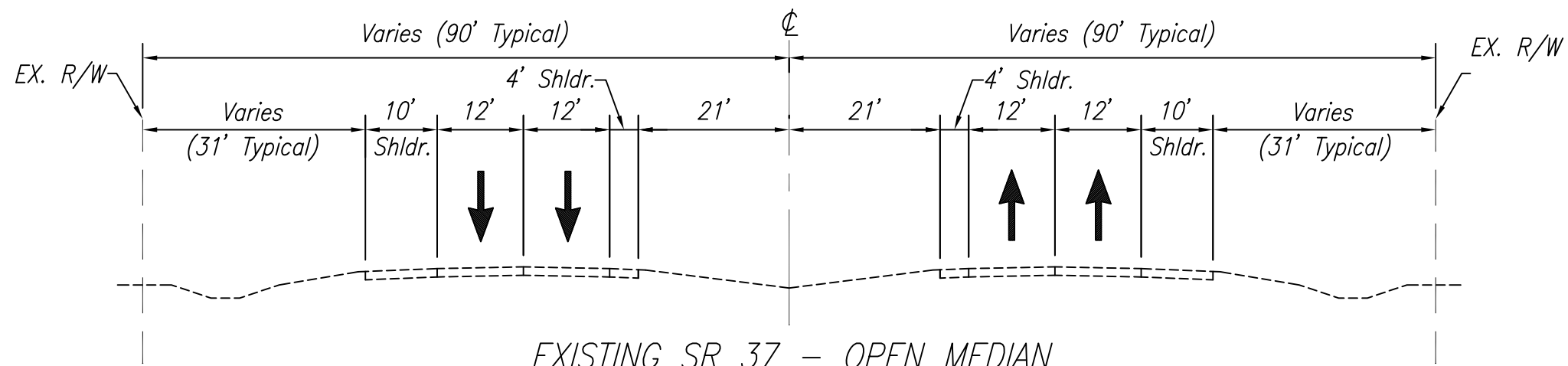
Table 3 below indicates the priority for construction of the proposed improvements. The ranking as shown generally flows south to north but can be revised without affecting the integrity of constructing methodologies.

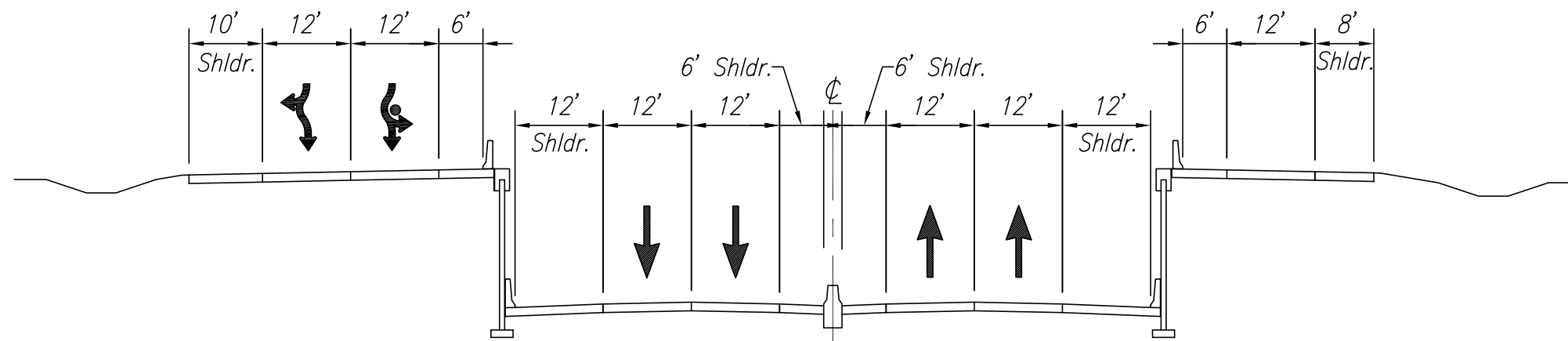
Table 3 – Construction Priorities		
Priority Rank	Binder Number	Intersection
1.	5	SR 37 at 146 th Street
2.	10	146 th Street at Allisonville Road
3.	1	SR 37 at 126 th Street
4.	2	SR 37 at 131 st Street
5.	3	SR 37 at 135 th Street
6.	4	SR 37 at 141 st Street
7.	6	SR 37 at Greenfield Avenue
8.	7	SR 37 at Town and Country Boulevard
9.	8	SR 37 at Pleasant Street
10.	9	SR 37 at SR 32 / SR 38



XV. PROJECT BUDGET

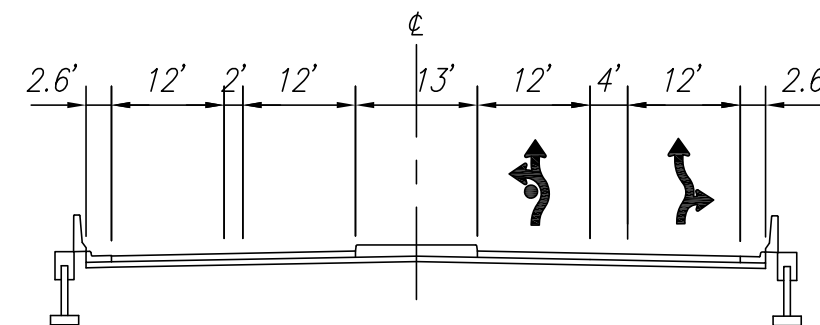
At the intersection of SR 37 and Town and Country Boulevard, a teardrop roundabout interchange is proposed, with a 4-lane bridge crossing SR 37. In order to construct these improvements, it is anticipated that construction cost will be \$25,933,795 in year 2025.





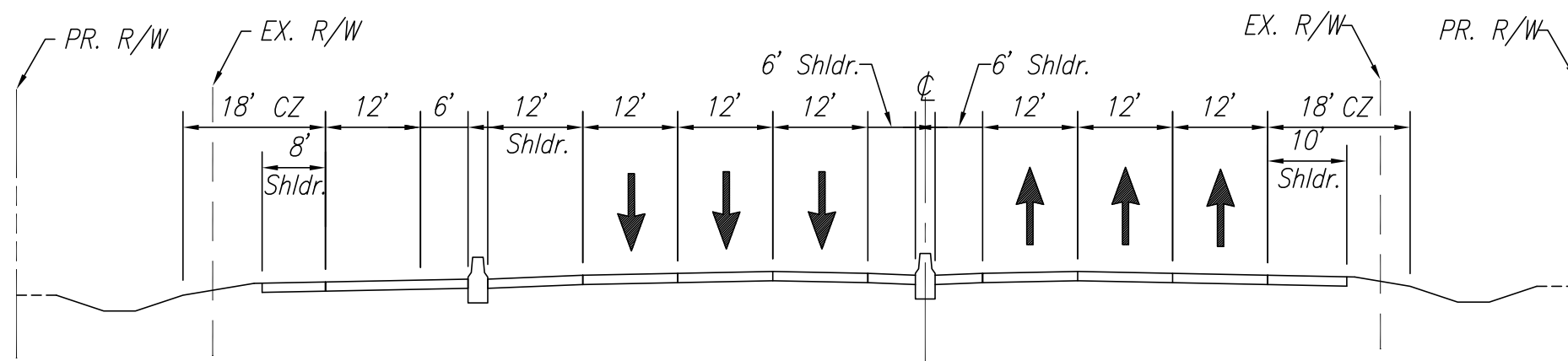
PROPOSED SR 37 – WITH CD LANES

Immediately North of Town and Country Blvd.



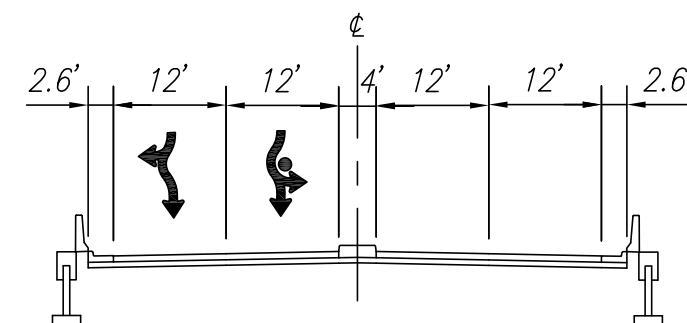
PROPOSED Town and Country Blvd.

East leg approaching intersection



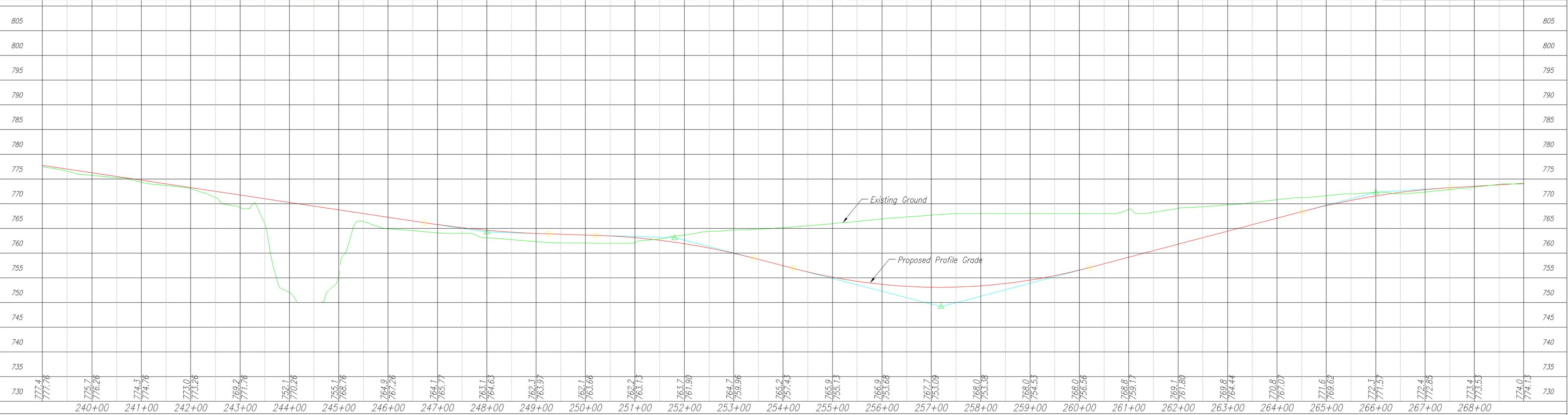
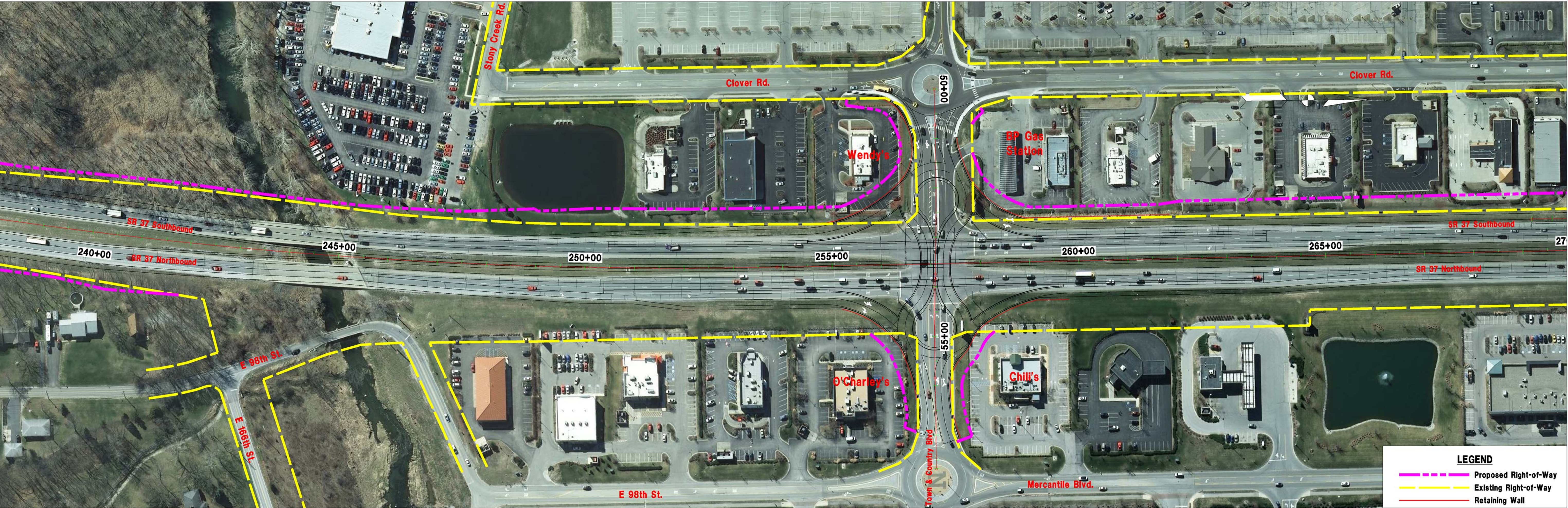
PROPOSED SR 37 – OPEN MEDIAN

North of Town and Country Blvd.



PROPOSED Town and Country Blvd.

East leg departing intersection

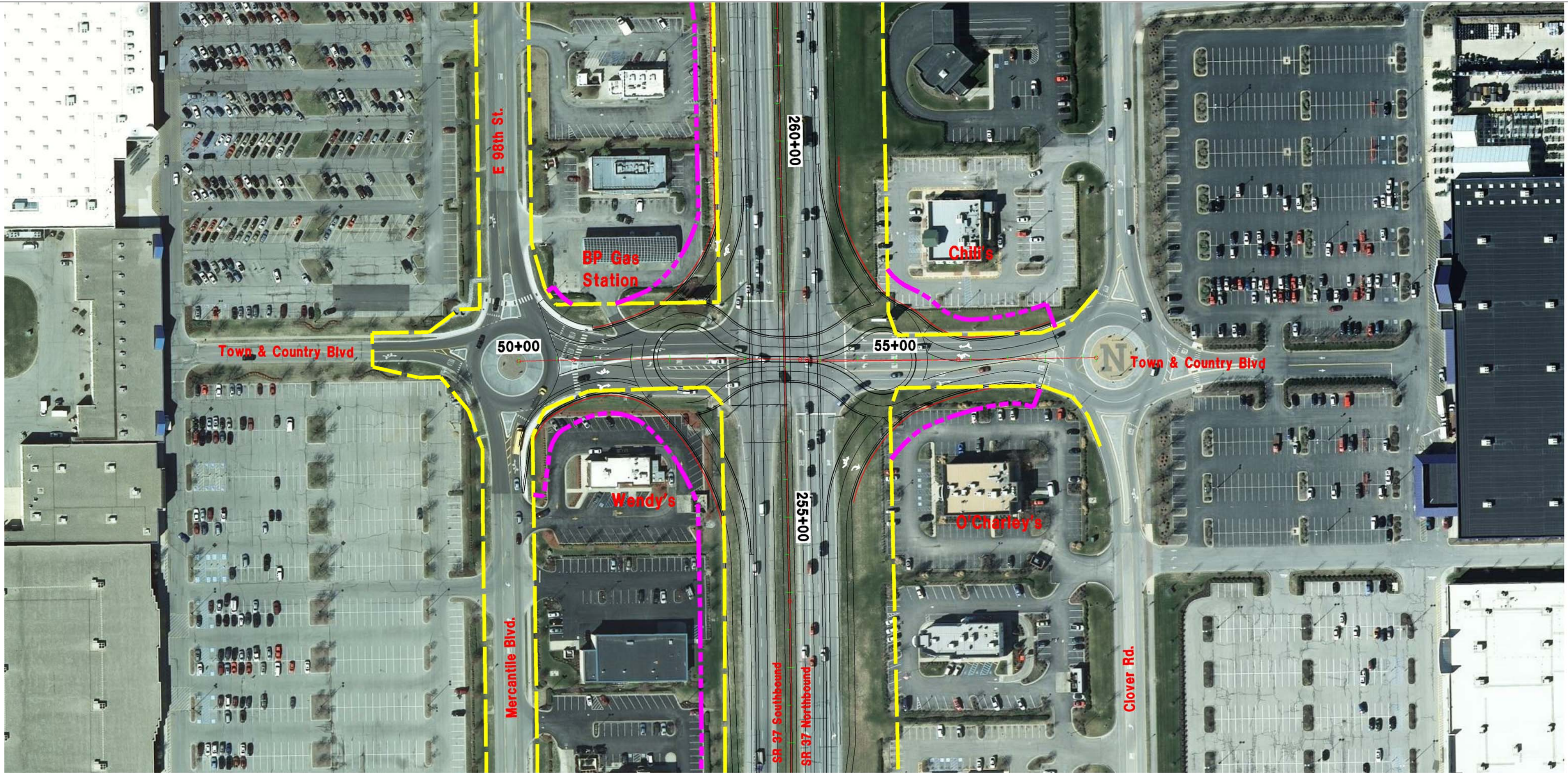




SR 37 MOBILITY STUDY

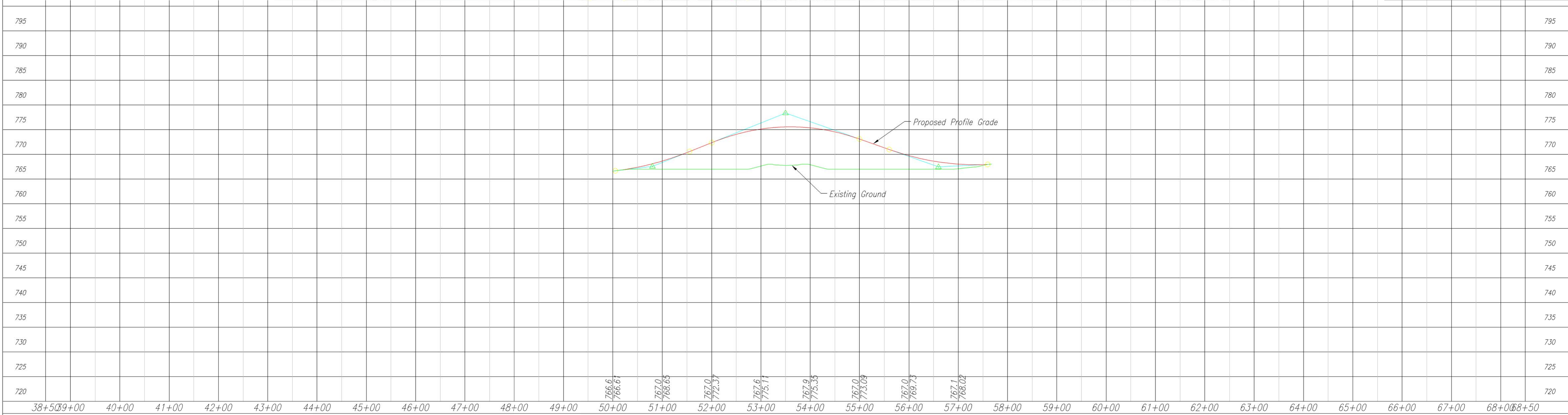


**PROPOSED TEARDROP
ROUNDAABOUT INTERCHANGE
SR 37 & TOWN & COUNTRY BOULEVARD**



LEGEND

- Proposed Right-of-Way
- Existing Right-of-Way
- Retaining Wall



SR 37 MOBILITY STUDY

UNITED
Consulting

AMERICAN
STRUCTUREPOINT
INC.

**PROPOSED TEARDROP
ROUNDAABOUT INTERCHANGE
TOWN & COUNTRY BOULEVARD & SR 37**



Town & Country Boulevard Project Development Cost Summary

SR 37 MOBILITY STUDY

Hamilton County, Town of Fishers and City of Noblesville

S.R. 37 from South of 126th Street to North of SR 38/32

PROJECT ITEMS:		PROJECT COST (IN YEAR OF EXPENDITURE)
TOWN & COUNTRY BOULEVARD		
Engineering Costs	\$	3,698,159
Construction Costs	\$	25,933,795
Construction Cost Contingencies	\$	2,593,380
Construction Inspection Costs	\$	3,890,069
Utility Relocation Cost	\$	-
Land Cost	\$	2,844,590
Subtotal Town & Country Boulevard Interchange		\$35,261,834

* The Town & Country Blvd Interchange is projected to be constructed in 2025. An inflation factor of 1.513 has been applied to obtain the construction cost shown in this table

TOTAL INTERCHANGE COST: \$35,261,834



Town & Country Boulevard Avenue Construction Cost Summary

SR 37 MOBILITY STUDY

Hamilton County, Town of Fishers and City of Noblesville
S.R. 37 from South of 126th Street to North of SR 38/32

MAJOR ELEMENT	BASE YEAR CONSTRUCTION COST (2012)
ROADWAY	\$ 11,330,645
GEOTECHNICAL MITIGATION	\$ 500,000
BRIDGE (Town & Country Boulevard Over S.R. 37)	\$ 2,750,000
BRIDGE (S.R. 37 Over Stony Creek)	\$ 1,000,000
LIFT STATION	\$ 1,560,000
TOTAL CONSTRUCTION COST:	\$ 17,140,645



ROAD ESTIMATE

PRICING REPORT

Date: 12/19/2012
Time: 14:23:52

Project: **SR 37 Mobility Study - Town and Country**
Location: **Town and Country Interchange**
County: **HAMILTON**
District: **Crawfordsville**

Project ID: **10-703 (7)**
Bid Date: **/ /** State: **IN**
Route: **SR 37**

Sect	Pay Item	Description	Quantity	Unit	Bid Price	Extension	Alt
100	105-06845	construction engineering	1.000	L.S.	303,499.41	303,499.41	
100	110-01001	mobilization and demobilization	1.000	L.S.	505,832.35	505,832.35	
GENERAL PROVISIONS SUBTOTALS						809,331.76	7.1%
200	201-52370	clearing right of way	1.000	L.S.	159,910.46	159,910.46	
200	202-02273	center curb, concrete, remove	71.000	SYS	16.61	1,179.31	
200	202-02279	curb and gutter, remove	703.000	L.F.	4.62	3,247.86	
200	202-52710	sidewalk, concrete, remove	120.000	SYS	7.88	945.60	
200	202-93999	signal pole, remove	4.000	EACH	495.00	1,980.00	
200	203-02000	excavation, common	77,013.000	C.Y.	7.88	606,862.44	
200	205-06931	temporary check dam, revetment riprap	325.000	TON	38.84	12,623.00	
200	205-06937	temporary silt fence	500.000	L.F.	1.74	870.00	
200	207-08263	subgrade treatment, type ia	41,892.000	SYS	6.24	261,406.08	
200	211-09194	b borrow	31,398.000	TON	34.00	1,067,532.00	
200	211-09264	structural backfill, type 1	829.000	C.Y.	23.88	19,796.52	
200	211-09266	structural backfill, type 3	25,506.000	C.Y.	21.27	542,512.62	
EARTHWORK SUBTOTALS						2,678,865.89	23.6%
300	301-07448	compacted aggregate, no. 53, base	3,125.000	TON	15.66	48,937.50	
300	302-06464	subbase for pccp	10,473.000	C.Y.	28.39	297,328.47	
300	303-01180	compacted aggregate, no. 53	2,089.000	TON	17.20	35,930.80	
300	306-08034	milling, asphalt, 1 1/2 in	1,007.000	SYS	1.30	1,309.10	
AGGREGATE PAVEMENT AND BASES SUBTOTALS						383,505.87	3.4%
400	401-07328	qc/qa-hma, 3, 70, surface, 9.5 mm	83.000	TON	72.77	6,039.91	
400	402-10084	hma for temporary pavement, b	5,192.000	TON	50.00	259,600.00	
ASPHALT PAVEMENT SUBTOTALS						265,639.91	2.3%
500	501-06266	profilograph, pccp	1.000	L.S.	15,000.00	15,000.00	
500	501-06323	qc/qa-pccp, 12 in	44,362.000	SYS	70.00	3,105,340.00	
500	503-05240	d-1 contraction joint	22,181.000	L.F.	9.19	203,843.39	
CONCRETE PAVEMENT SUBTOTALS						3,324,183.39	29.3%
600	601-01522	guardrail, transition type tqb	3.000	EACH	1,978.24	5,934.72	
600	601-94689	guardrail end treatment, os	3.000	EACH	2,530.55	7,591.65	
600	601-99105	guardrail, w-beam, 6 ft 3 in spacing	1,668.000	L.F.	17.42	29,056.56	
600	602-06729	barrier delineator	51.000	EACH	12.49	636.99	
600	602-08603	concrete barrier, 45 in	1,991.000	L.F.	91.00	181,181.00	
600	603-06040	fence, farm field, 47 in	5,418.000	L.F.	5.50	29,799.00	
600	604-07569	pavers {pavers}	491.000	SYS	827.77	406,435.07	
600	605-06120	curb, concrete	619.000	L.F.	23.58	14,596.02	
600	605-06140	curb and gutter, concrete	1,398.000	L.F.	14.29	19,977.42	
600	605-06145	curb and gutter, b, concrete	846.000	L.F.	14.17	11,987.82	
600	605-06255	center curb, d, concrete	422.000	SYS	48.55	20,488.10	
600	615-06510	monument, c	5.000	EACH	419.32	2,096.60	

PRICING REPORT

Date: 12/19/2012
Time: 14:23:53

Project: **SR 37 Mobility Study - Town and Country**
Location: **Town and Country Interchange**
County: **HAMILTON**
District: **Crawfordsville**

Project ID: **10-703 (7)**
Bid Date: **/ /** State: **IN**
Route: **SR 37**

Sect	Pay Item	Description	Quantity	Unit	Bid Price	Extension	Alt
600	615-06515	monument, d	20.000	EACH	141.25	2,825.00	
600	616-02320	geotextiles	446.000	SYS	2.51	1,119.46	
600	616-06405	riprap, revetment	251.000	TON	29.36	7,369.36	
600	621-01004	mobilization and demobilization for seeding	4.000	EACH	382.61	1,530.44	
600	621-06545	fertilizer	4.000	TON	327.69	1,310.76	
600	621-06554	seed mixture, u	864.000	LBS	5.62	4,855.68	
600	621-06557	seed mixture, t	381.000	LBS	2.15	819.15	
600	621-06565	mulching material	16.000	TON	305.97	4,895.52	
600	621-06567	water	12.000	M.G.	3.74	44.88	
600	621-06574	sodding	2,857.000	SYS	3.12	8,913.84	
600	628-09403	field office, c	18.000	MONTH	2,082.44	37,483.92	
600	628-11068	cellular telephone/radio	2.000	EACH	150.38	300.76	
600	628-11069	cellular telephone/radio service, anytime minutes {cell phone}	36.000	MONTH	112.11	4,035.96	

INCIDENTAL CONSTRUCTION SUBTOTALS

805,285.68
7.1%

700	701-90386	temporary sheet piling	1.000	L.S.	308,925.00	308,925.00	
700	706-08496	reinforced concrete moment slab, 12 in	2,982.000	SYS	87.86	261,998.52	
700	706-09545	coarse aggregate, no 8	746.000	C.Y.	61.00	45,506.00	
700	706-09959	railing, concrete, ft	6,706.000	L.F.	60.00	402,360.00	
700	715-05048	pipe, type 4 circular 6 in	12,099.000	L.F.	3.24	39,200.76	
700	715-05053	pipe, underdrain, outlet 6 in	270.000	L.F.	11.77	3,177.90	
700	715-05149	pipe, type 2 circular 12 in	5,593.000	L.F.	29.00	162,197.00	
700	715-09064	video inspection for pipe	5,593.000	L.F.	1.48	8,277.64	
700	718-06528	outlet protector, 1	10.000	EACH	519.56	5,195.60	
700	718-06532	video inspection for underdrains	3,000.000	L.F.	0.94	2,820.00	
700	718-52610	aggregate for underdrains	1,089.000	C.Y.	32.72	35,632.08	
700	718-99153	geotextiles for underdrain	8,291.000	SYS	0.98	8,125.18	
700	720-07300	inlet, type h, with slotted drain	12.000	EACH	4,502.79	54,033.48	
700	720-07302	inlet, type ha, with slotted drain	12.000	EACH	1,757.88	21,094.56	
700	720-45410	manhole, c4	12.000	EACH	2,000.00	24,000.00	
700	720-98174	inlet, b15	12.000	EACH	2,189.87	26,278.44	
700	720-98555	inlet, c15	12.000	EACH	2,161.77	25,941.24	
700	731-93945	face panels, concrete	58,984.000	S.F.	11.99	707,218.16	
700	731-93946	wall erection	58,984.000	S.F.	5.56	327,951.04	
700	731-93947	leveling pad, concrete	4,336.000	L.F.	22.00	95,392.00	

STRUCTURES SUBTOTALS

2,565,324.60
22.6%

800	801-01093	temporary worksite speed limit sign assembly	4.000	EACH	723.00	2,892.00	
800	801-03290	construction sign, c	2.000	EACH	199.19	398.38	
800	801-04308	road closure sign assembly	4.000	EACH	308.61	1,234.44	
800	801-06625	detour route marker assembly	18.000	EACH	98.84	1,779.12	
800	801-06640	construction sign, a	24.000	EACH	160.87	3,860.88	
800	801-06645	construction sign, b	4.000	EACH	58.33	233.32	

PRICING REPORTDate: 12/19/2012
Time: 14:23:53Project: **SR 37 Mobility Study - Town and Country**
Location: **Town and Country Interchange**
County: **HAMILTON**
District: **Crawfordsville**Project ID: **10-703 (7)**
Bid Date: **/ /** State: **IN**
Route: **SR 37**

Sect	Pay Item	Description	Quantity	Unit	Bid Price	Extension	Alt
800	801-06710	flashing arrow sign	510.000	DAY	8.52	4,345.20	
800	801-06775	maintaining traffic	1.000	L.S.	202,332.94	202,332.94	
800	801-07024	energy absorbing terminal, cz, tl-3	1.000	EACH	7,316.67	7,316.67	
800	801-07118	barricade, iii-a	228.000	L.F.	13.17	3,002.76	
800	801-07119	barricade, iii-b	48.000	L.F.	14.08	675.84	
800	801-08400	temporary traffic barrier, type 1	3,009.000	L.F.	16.86	50,731.74	
800	801-08507	temporary traffic barrier, type 1, anchored	296.000	L.F.	34.09	10,090.64	
800	801-08508	temporary traffic barrier, type 2, anchored	3,009.000	L.F.	25.00	75,225.00	
800	801-09133	temporary changeable message sign	2.000	EACH	6,193.01	12,386.02	
800	801-52817	temporav crossover, b	2.000	EACH	25,000.00	50,000.00	
800	802-05701	sign post, square, type 1, reinforced anchor base	340.000	L.F.	12.95	4,403.00	
800	802-07057	sign, panel, with legend	429.000	S.F.	14.81	6,353.49	
800	802-07138	wide flange sign post support foundation, ix	2.000	EACH	242.00	484.00	
800	802-07159	cantilever sign support foundation, ii	2.000	EACH	3,349.33	6,698.66	
800	802-09840	sign, sheet, with legend 0.100 in thickness	115.000	S.F.	17.27	1,986.05	
800	802-76095	structural steel, breakaway	681.000	LBS	2.68	1,825.08	
800	802-76135	overhead sign structure, cantilever single arm	1.000	EACH	20,672.20	20,672.20	
800	804-06770	delineator post	10.000	EACH	43.14	431.40	
800	805-02087	transportation of salvageable signal equipment	1.000	L.S.	495.57	495.57	
800	808-10031	line, multi-component, broken, white, 4 in	2,528.000	L.F.	0.43	1,087.04	
800	808-10033	line, multi-component, solid, white, 4 in	9,695.000	L.F.	0.46	4,459.70	
800	808-10034	line, multi-component, solid, yellow, 4 in	9,328.000	L.F.	0.46	4,290.88	
800	808-10037	line, multi-component, solid, white, 8 in	3,816.000	L.F.	1.04	3,968.64	
800	808-75071	pavement message marking, preformed plastic, lane indication arrow	8.000	EACH	187.00	1,496.00	
800	808-75510	transverse markings, preformed plastic, crosshatch line, white, 24 in	538.000	L.F.	12.09	6,504.42	
800	808-75998	snowplowable raised pavement marker	352.000	EACH	19.45	6,846.40	

TRAFFIC CONTROL DEVICES AND LIGHTING SUBTOTALS**498,507.48**
4.4%**TOTALS****11,330,644.58**
100.0%

BRIDGE ESTIMATE

PRICING REPORT

JTB 11/26/12
JEC 11/26/12

Date: 11/26/2012
Time: 15:24:04

Project: **Town&Country over SR 37 - Concrete Bridge Option**
Location: **Hamilton County**
County: **HAMILTON**
District: **Greenfield**

Project ID: **10-703-TOWN&COUNTRY**
Bid Date: **/ /** State: **IN**
Route:

Pay Item	Description	Quantity	Unit	Bid Price	Extension	Alt
105-06845	construction engineering	1.000	L.S.	75,361.00	75,361.00	
110-01001	mobilization and demobilization	1.000	L.S.	125,601.00	125,601.00	
203-02020	excavation, unclassified	557.000	C.Y.	20.83	11,602.31	
211-02050	b borrow	557.000	C.Y.	27.42	15,272.94	
302-07455	dense graded subbase	156.000	C.Y.	62.94	9,818.64	
609-06259	reinforced concrete bridge approach 12 in	939.000	SYS	83.33	78,246.87	
701-06011	dynamic pile load test	3.000	EACH	1,651.34	4,954.02	
701-08122	pile, steel pipe, 0.375", 14	5,580.000	L.F.	42.87	239,214.60	
701-09559	test pile, dynamic, restrike	3.000	EACH	1,317.82	3,953.46	
701-09690	test pile, dynamic, 14 in non-production	210.000	L.F.	42.87	9,002.70	
702-51005	concrete, a, substructure	300.000	C.Y.	584.17	175,251.00	
702-51015	concrete, b, footings	206.000	C.Y.	307.53	63,351.18	
703-06028	reinforcing bars	61,660.000	LBS	0.91	56,110.60	
703-06029	reinforcing bars, epoxy coated	443,858.000	LBS	0.95	421,665.10	
704-51002	concrete, c, superstructure	1,696.000	C.Y.	560.34	950,336.64	
706-09959	railing, concrete, ft	236.000	L.F.	64.42	15,203.12	
707-05983	structural member, concrete i-beam, 36 in x 12 in	2,680.000	L.F.	160.01	428,826.80	
709-51821	surface seal	1.000	L.S.	29,198.00	29,198.00	

TOTALS

2,712,969.98

PRICING REPORT

JTB 11/26/12
JEC 11/26/12

Date: 11/26/2012
Time: 17:12:56

Project: **SR 37 over Stoney Creek - Widening**
Location: **Hamilton County**
County: **HAMILTON**
District: **Greenfield**

Project ID: **10-703-SR37 OVER STO**
Bid Date: **/ /** State: **IN**
Route:

Pay Item	Description	Quantity	Unit	Bid Price	Extension	Alt
105-06845	construction engineering	1.000	L.S.	16,508.00	16,508.00	
110-01001	mobilization and demobilization	1.000	L.S.	41,270.00	41,270.00	
202-51328	present structure, remove portions	1.000	L.S.	40,000.00	40,000.00	
206-51220	excavation, wet	118.700	C.Y.	100.00	11,870.00	
302-07455	dense graded subbase	51.100	C.Y.	62.94	3,216.23	
609-06257	reinforced concrete bridge approach, 10 in	306.000	SYS	89.10	27,264.60	
701-91792	pile tip, steel h	12.000	EACH	119.00	1,428.00	
701-95782	pile, steel h, hp 12 x 84	180.000	L.F.	72.00	12,960.00	
702-51005	concrete, a, substructure	90.300	C.Y.	584.17	52,750.55	
702-51015	concrete, b, footings	32.400	C.Y.	307.53	9,963.97	
702-51110	grates, basins, and fittings, cast iron	1,734.000	LBS	4.50	7,803.00	
702-92857	concrete, c, substructure	56.900	C.Y.	560.00	31,864.00	
703-06028	reinforcing bars	15,303.000	LBS	0.91	13,925.73	
703-06029	reinforcing bars, epoxy coated	97,845.000	LBS	0.95	92,952.75	
704-51002	concrete, c, superstructure	250.400	C.Y.	560.00	140,224.00	
706-09960	railing, concrete, fc	392.000	L.F.	70.00	27,440.00	
709-51821	surface seal	1.000	L.S.	12,705.00	12,705.00	
711-51038	structural steel	1.000	L.S.	223,616.00	223,616.00	
724-51925	structural expansion joint, ss	263.200	L.F.	170.00	44,744.00	
726-11448	bearing assembly, expansion, type 1	15.000	EACH	500.00	7,500.00	
726-11451	bearing assembly, fixed, type 1	5.000	EACH	500.00	2,500.00	
801-06203	temporary pavement marking, 4 in	1,533.000	L.F.	0.46	705.18	
801-06710	flashing arrow sign	480.000	DAY	9.00	4,320.00	
801-06775	maintaining traffic	1.000	L.S.	20,000.00	20,000.00	
801-08508	temporary traffic barrier, type 2, anchored	933.000	L.F.	38.20	35,640.60	

TOTALS

883,171.61

LIFT STATION ESTIMATE

Town & Country Boulevard Lift Station Cost Summary

GENERAL INFORMATION

Intersection	Town & Country Boulevard and S.R. 37
Station (of Lift Station)	257+09.73

DESIGN INFORMATION

Storm Sewer Inflow Elev	749.1
Length to Outfall (ft)	1442.0
Outfall Elevation	750.00
High Point Station	226+74.43
High Point Elevation	794.77
Revised Outfall Elevation	790.77
Revised Distance to Outfall	3035.0
Drainage Area (ac)	2.44
10-Year Flow Q_{10} (cfs)	10.50
50-Year Flow Q_{50} (cfs)	13.04
100-Year Flow Q_{100} (cfs)	14.12

CONSTRUCTION COST

Estimated Lift Station Construction Cost	\$ 950,000.00
Estimated Force Main Construction Cost	\$ 610,000.00
Total Estimated Construction Cost	\$ 1,560,000.00

OPERATION & MAINTENANCE COST

Operation	\$ 10,000.00
Maintenance	\$ 15,000.00
Equipment Replacement	\$ 15,000.00
Total Annual OM&R Cost	\$ 40,000.00

ROAD QUANTITIES

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> </u>	<u> <i>3/27/12</i> </u>	Checked By: <u> <i>ATW</i> </u>
105-06845	CONSTRUCTION ENGINEERING	1 LS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>3/27/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
105-06845	CONSTRUCTION ENGINEERING	1 LS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> </u>	<u> <i>3/27/12</i> </u>	Checked By: <u> <i>ATW</i> </u>
105-06845	CONSTRUCTION ENGINEERING	1 LS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>3/27/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
105-06845	CONSTRUCTION ENGINEERING	1 LS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> </u>	<u> <i>3/27/12</i> </u>	Checked By: <u> <i>ATW</i> </u>
105-06845	CONSTRUCTION ENGINEERING	1 LS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>3/27/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
105-06845	CONSTRUCTION ENGINEERING	1 LS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> </u>	<u> <i>3/27/12</i> </u>	Checked By: <u> <i>ATW</i> </u>
105-06845	CONSTRUCTION ENGINEERING	1 LS

[illegible]

						TOTAL =	1.0
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10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: BWS 3/27/12

Checked By: ATW 11/24/12

110-01001

MOBILIZATION AND DEMOBILIZATION

1
LS

[illegible]

<i>TOTAL</i>	<i>1.0</i>
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<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>3/27/12</i> </u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i> </u>	
201-52370	CLEARING RIGHT OF WAY	1 LS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>3/27/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
201-52370	CLEARING RIGHT OF WAY	1 LS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>3/27/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
201-52370	CLEARING RIGHT OF WAY	1 LS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>3/27/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
201-52370	CLEARING RIGHT OF WAY	1 LS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>3/27/12</i> </u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i> </u>	
201-52370	CLEARING RIGHT OF WAY	1 LS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>3/27/12</i> </u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i> </u>	
201-52370	CLEARING RIGHT OF WAY	1 LS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>3/27/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
201-52370	CLEARING RIGHT OF WAY	1 LS

[illegible]

					<i>TOTAL =</i>	<i>1.0</i>
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<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
202-02279	CURB AND GUTTER, REMOVE	703 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
202-02279	CURB AND GUTTER, REMOVE	703 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
202-02279	CURB AND GUTTER, REMOVE	703 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
202-02279	CURB AND GUTTER, REMOVE	703 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
202-02279	CURB AND GUTTER, REMOVE	703 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
202-02279	CURB AND GUTTER, REMOVE	703 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
202-02279	CURB AND GUTTER, REMOVE	703 LFT

[illegible]

TOTAL = 703.0

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/17/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
202-93999	SIGNAL POLE, REMOVE	4 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/17/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
202-93999	SIGNAL POLE, REMOVE	4 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/17/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
202-93999	SIGNAL POLE, REMOVE	4 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/17/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
202-93999	SIGNAL POLE, REMOVE	4 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/17/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
202-93999	SIGNAL POLE, REMOVE	4 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/17/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
202-93999	SIGNAL POLE, REMOVE	4 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/17/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
202-93999	SIGNAL POLE, REMOVE	4 EACH

[illegible]

					<i>TOTAL =</i>	<i>4.0</i>
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<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
<i>By:</i> <u> <i>BWS</i> <i>10/30/12</i> </u>	<i>Checked By:</i> <u> <i>srp</i> </u>	<i>11/26/12</i>
203-02000	EXCAVATION, COMMON	77,013 CYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
<i>By:</i> <u> <i>BWS</i> <i>10/30/12</i> </u>	<i>Checked By:</i> <u> <i>srp</i> </u>	<i>11/26/12</i>
203-02000	EXCAVATION, COMMON	77,013 CYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
<i>By:</i> <u> <i>BWS</i> <i>10/30/12</i> </u>	<i>Checked By:</i> <u> <i>srp</i> </u>	<i>11/26/12</i>
203-02000	EXCAVATION, COMMON	77,013 CYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
<i>By:</i> <u> <i>BWS</i> <i>10/30/12</i> </u>	<i>Checked By:</i> <u> <i>srp</i> <i>11/26/12</i> </u>	
203-02000	EXCAVATION, COMMON	77,013 CYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
<i>By:</i> <u> <i>BWS</i> <i>10/30/12</i> </u>	<i>Checked By:</i> <u> <i>srp</i> <i>11/26/12</i> </u>	
203-02000	EXCAVATION, COMMON	77,013 CYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
<i>By:</i> <u> <i>BWS</i> <i>10/30/12</i> </u>	<i>Checked By:</i> <u> <i>srp</i> <i>11/26/12</i> </u>	
203-02000	EXCAVATION, COMMON	77,013 CYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
<i>By:</i> <u> <i>BWS</i> <i>10/30/12</i> </u>	<i>Checked By:</i> <u> <i>srp</i> <i>11/26/12</i> </u>	
203-02000	EXCAVATION, COMMON	77,013 CYS

[illegible]

	<i>TOTAL =</i>	77013.0
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<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u>MAC</u> <u>5/29/12</u>	Checked By: <u>JPS</u> <u>11/21/12</u>	
205-06931	TEMPORARY CHECK DAM, REVETMENT RIPRAP	325 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u>MAC</u>	<u>5/29/12</u>	Checked By: <u>JPS</u>
205-06931	TEMPORARY CHECK DAM, REVETMENT RIPRAP	325 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u>MAC</u>	<u>5/29/12</u>	Checked By: <u>JPS</u>
205-06931	TEMPORARY CHECK DAM, REVETMENT RIPRAP	325 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u>MAC</u>	<u>5/29/12</u>	Checked By: <u>JPS</u>
205-06931	TEMPORARY CHECK DAM, REVETMENT RIPRAP	325 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u>MAC</u>	<u>5/29/12</u>	Checked By: <u>JPS</u>
205-06931	TEMPORARY CHECK DAM, REVETMENT RIPRAP	325 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u>MAC</u>	<u>5/29/12</u>	Checked By: <u>JPS</u>
205-06931	TEMPORARY CHECK DAM, REVETMENT RIPRAP	325 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u>MAC</u>	<u>5/29/12</u>	Checked By: <u>JPS</u>
205-06931	TEMPORARY CHECK DAM, REVETMENT RIPRAP	325 TON

[illegible]

	<i>TOTAL =</i>	325.0
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<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u>MAC</u>	<u>5/10/12</u>	Checked By: <u>JPS</u>
205-06937	TEMPORARY SILT FENCE	500 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u>MAC</u>	<u>5/10/12</u>	Checked By: <u>JPS</u>
205-06937	TEMPORARY SILT FENCE	500 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u>MAC</u>	<u>5/10/12</u>	Checked By: <u>JPS</u>
205-06937	TEMPORARY SILT FENCE	500 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u>MAC</u>	<u>5/10/12</u>	Checked By: <u>JPS</u>
205-06937	TEMPORARY SILT FENCE	500 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u>MAC</u>	<u>5/10/12</u>	Checked By: <u>JPS</u>
205-06937	TEMPORARY SILT FENCE	500 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u>MAC</u>	<u>5/10/12</u>	Checked By: <u>JPS</u>
205-06937	TEMPORARY SILT FENCE	500 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u>MAC</u>	<u>5/10/12</u>	Checked By: <u>JPS</u>
205-06937	TEMPORARY SILT FENCE	500 LFT

[illegible]

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: BWS 4/13/12Checked By: ATW 11/25/12**207-08263****SUBGRADE TREATMENT, TYPE IA****41,892
SYS**

<i>Begin Station</i>	<i>End Station</i>	<i>Begin Width</i>	<i>End Width</i>	<i>Area (sft)</i>		<i>Area (sys)</i>
Pavement Area copied from 501-06323:						44361.3
Line "S-2-A"						
Line "A"						
238+33.71	240+22.16	Rt	2.00	376.90		41.9
240+22.16	245+01.18	Rt	2.00	958.04		106.4
245+01.18	245+54.43	Rt	2.00	106.50		11.8
245+54.43	249+91.75	Rt	2.00	874.64		97.2
249+91.75	250+38.72	Rt	2.00	93.94		10.4
250+38.72	263+73.31	Rt	2.00	2669.18		296.6
263+73.31	266+70.35	Rt	2.00	594.08		66.0
266+70.35	268+42.63	Rt	2.00	344.56		38.3
238+33.71	240+22.57	Lt	2.00	377.72		42.0
240+22.57	245+54.43	Lt	2.00	1063.72		118.2
245+54.43	246+03.03	Lt	2.00	97.20		10.8
246+03.03	247+49.27	Lt	2.00	292.48		32.5
247+49.27	250+46.46	Lt	2.00	594.38		66.0
250+46.46	263+81.00	Lt	2.00	2669.08		296.6
263+81.00	268+42.63	Lt	2.00	923.26		102.6
Line "TC_SE"						
10+00.00	10+60.24	Rt	2.00	120.48		13.4
10+60.24	11+59.99	Rt	2.00	199.50		22.2
11+59.99	14+56.18	Rt	2.00	592.38		65.8
14+56.18	14+89.01	Rt	2.00	65.66		7.3
14+89.01	16+11.02	Rt	2.00	244.02		27.1
Line "TC_SW"						
20+00.00	21+62.90	Lt	2.00	325.80		36.2
21+62.90	22+04.64	Lt	2.00	83.48		9.3
22+04.64	23+07.67	Lt	2.00	206.06		22.9
23+07.67	26+07.64	Lt	2.00	599.94		66.7
Line "TC_NW"						
40+00.00	44+54.18	Lt	2.00	908.36		100.9
44+54.18	44+87.02	Lt	2.00	65.68		7.3
44+87.02	46+09.02	Lt	2.00	244.00		27.1
Line "TC_NE"						
30+00.00	31+54.71	Rt	2.00	309.42		34.4
31+54.71	32+04.64	Rt	2.00	99.86		11.1
32+04.64	36+09.51	Rt	2.00	809.74		90.0

SUBTOTAL (THIS PAGE) =**46240.2**

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>SRS</i> <i>11/16/12</i></u>	Checked By: <u> <i>srp</i> <i>11/26/12</i></u>	
211-09194	B BORROW	31,398 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>SRS</i> </u>	<u> <i>11/16/12</i> </u>	Checked By: <u> <i>srp</i> </u>
211-09194	B BORROW	31,398 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>SRS</i> <i>11/16/12</i></u>	Checked By: <u> <i>srp</i> <i>11/26/12</i></u>	
211-09194	B BORROW	31,398 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>SRS</i> <i>11/16/12</i></u>	Checked By: <u> <i>srp</i> <i>11/26/12</i></u>	
211-09194	B BORROW	31,398 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>SRS</i> <i>11/16/12</i></u>	Checked By: <u> <i>srp</i> <i>11/26/12</i></u>	
211-09194	B BORROW	31,398 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>SRS</i> <i>11/16/12</i></u>	Checked By: <u> <i>srp</i> <i>11/26/12</i></u>	
211-09194	B BORROW	31,398 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>SRS</i> <i>11/16/12</i></u>	Checked By: <u> <i>srp</i> <i>11/26/12</i></u>	
211-09194	B BORROW	31,398 TON

[illegible]

<i>TOTAL =</i>		<i>31397.7</i>
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SR 37 MOBILITY STUDY
Town and Country Boulevard

Checked By: BWC 11/24/12

829
CYS

***TOTAL* = 828.6**

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: SRS 10/22/12

Checked By: srp 11/26/12

**25,506
CYS**

[illegible]

<i>TOTAL =</i>	3612.7
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SR 37 MOBILITY STUDY
Town and Country Boulevard

Checked By: srp 11/22/12

CYS

[illegible]

TOTAL = 25505.8

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: BWS 4/23/12

Checked By: BWC 11/24/12

301-07448

COMPACTED AGGREGATE, NO. 53, BASE

**3,125
TON**

[illegible]

TOTAL =	3124.7
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10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: BWS 4/11/12Checked By: ATW 11/25/12**302-06464****SUBBASE FOR PCCP****10,473
CYS**

<i>Begin Station</i>	<i>End Station</i>	<i>Side</i>	<i>Width</i>	<i>Area (sft)</i>	<i>Depth (ft)</i>	<i>Volume (cys)</i>
Pavement Area copied from 501-06323 multiplied by 9:				399251.39	0.75	11090.3
Outside Area (2' on either side):						
Line "A"						
238+33.71	240+22.16	Rt	2.00	376.90	0.75	10.5
240+22.16	245+01.18	Rt	2.00	958.04	0.75	26.6
245+01.18	245+54.43	Rt	2.00	106.50	0.75	3.0
245+54.43	249+91.75	Rt	2.00	874.64	0.75	24.3
249+91.75	250+38.72	Rt	2.00	93.94	0.75	2.6
250+38.72	263+73.31	Rt	2.00	2669.18	0.75	74.1
263+73.31	266+70.35	Rt	2.00	594.08	0.75	16.5
266+70.35	268+42.63	Rt	2.00	344.56	0.75	9.6
238+33.71	240+22.57	Lt	2.00	377.72	0.75	10.5
240+22.57	245+54.43	Lt	2.00	1063.72	0.75	29.5
245+54.43	246+03.03	Lt	2.00	97.20	0.75	2.7
246+03.03	247+49.27	Lt	2.00	292.48	0.75	8.1
247+49.27	250+46.46	Lt	2.00	594.38	0.75	16.5
250+46.46	263+81.00	Lt	2.00	2669.08	0.75	74.1
263+81.00	268+42.63	Lt	2.00	923.26	0.75	25.6
Line "TC_SE"						
10+00.00	10+60.24	Rt	2.00	120.48	0.75	3.3
10+60.24	11+59.99	Rt	2.00	199.50	0.75	5.5
11+59.99	14+56.18	Rt	2.00	592.38	0.75	16.5
14+56.18	14+89.01	Rt	2.00	65.66	0.75	1.8
14+89.01	16+11.02	Rt	2.00	244.02	0.75	6.8
Line "TC_SW"						
20+00.00	21+62.90	Lt	2.00	325.80	0.75	9.1
21+62.90	22+04.64	Lt	2.00	83.48	0.75	2.3
22+04.64	23+07.67	Lt	2.00	206.06	0.75	5.7
23+07.67	26+07.64	Lt	2.00	599.94	0.75	16.7
Line "TC_NW"						
40+00.00	44+54.18	Lt	2.00	908.36	0.75	25.2
44+54.18	44+87.02	Lt	2.00	65.68	0.75	1.8
44+87.02	46+09.02	Lt	2.00	244.00	0.75	6.8
Line "TC_NE"						
30+00.00	31+54.71	Rt	2.00	309.42	0.75	8.6
31+54.71	32+04.64	Rt	2.00	99.86	0.75	2.8
32+04.64	36+09.51	Rt	2.00	809.74	0.75	22.5

SUBTOTAL (THIS PAGE) = 11560.0

TOTAL = 10472.8

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS </u> <u> 4/17/12 </u></p>	<p>Checked By: <u> ATW </u> <u> 11/25/12 </u></p>	
<p>303-01180</p>	<p>COMPACTED AGGREGATE, NO. 53</p>	<p>2,089</p> <p>TON</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS </u> <u> 4/17/12 </u></p>	<p>Checked By: <u> ATW </u> <u> 11/25/12 </u></p>	
<p>303-01180</p>	<p>COMPACTED AGGREGATE, NO. 53</p>	<p>2,089</p> <p>TON</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS </u> <u> 4/17/12 </u></p>	<p>Checked By: <u> ATW </u> <u> 11/25/12 </u></p>	
<p>303-01180</p>	<p>COMPACTED AGGREGATE, NO. 53</p>	<p>2,089</p> <p>TON</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS </u> <u> 4/17/12 </u></p>	<p>Checked By: <u> ATW </u> <u> 11/25/12 </u></p>	
<p>303-01180</p>	<p>COMPACTED AGGREGATE, NO. 53</p>	<p>2,089</p> <p>TON</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS </u> <u> 4/17/12 </u></p>	<p>Checked By: <u> ATW </u> <u> 11/25/12 </u></p>	
<p>303-01180</p>	<p>COMPACTED AGGREGATE, NO. 53</p>	<p>2,089</p> <p>TON</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS 4/17/12 </u></p>	<p>Checked By: <u> ATW 11/25/12 </u></p>	
<p>303-01180</p>	<p>COMPACTED AGGREGATE, NO. 53</p>	<p>2,089</p> <p>TON</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS </u> <u> 4/17/12 </u></p>	<p>Checked By: <u> ATW </u> <u> 11/25/12 </u></p>	
<p>303-01180</p>	<p>COMPACTED AGGREGATE, NO. 53</p>	<p>2,089</p> <p>TON</p>

[illegible]

<i>TOTAL =</i>		<i>2088.2</i>
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<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u>ATW</u> <u>11/24/12</u>	Checked By: <u>BWS</u> <u>11/24/12</u>	
306-08034	MILLING, ASPHALT, 1 1/2 IN	1,007 SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u>ATW</u> <u>11/24/12</u>	Checked By: <u>BWS</u> <u>11/24/12</u>	
306-08034	MILLING, ASPHALT, 1 1/2 IN	1,007 SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u>ATW</u> <u>11/24/12</u>	Checked By: <u>BWS</u> <u>11/24/12</u>	
306-08034	MILLING, ASPHALT, 1 1/2 IN	1,007 SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u>ATW</u> <u>11/24/12</u>	Checked By: <u>BWS</u> <u>11/24/12</u>	
306-08034	MILLING, ASPHALT, 1 1/2 IN	1,007 SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u>ATW</u> <u>11/24/12</u>	Checked By: <u>BWS</u> <u>11/24/12</u>	
306-08034	MILLING, ASPHALT, 1 1/2 IN	1,007 SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u>ATW</u> <u>11/24/12</u>	Checked By: <u>BWS</u> <u>11/24/12</u>	
306-08034	MILLING, ASPHALT, 1 1/2 IN	1,007 SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u>ATW</u> <u>11/24/12</u>	Checked By: <u>BWS</u> <u>11/24/12</u>	
306-08034	MILLING, ASPHALT, 1 1/2 IN	1,007 SYS

[illegible]

<i>TOTAL =</i>		<i>1006.1</i>
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<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u>ATW</u> <u>11/24/12</u></p>	<p>Checked By: <u>BWS</u> <u>11/24/12</u></p>	
<p>401-07328</p>	<p>QC/QA-HMA, 3, 70, SURFACE, 9.5 mm</p>	<p>83</p> <p>TON</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u>ATW</u> <u>11/24/12</u></p>	<p>Checked By: <u>BWS</u> <u>11/24/12</u></p>	
<p>401-07328</p>	<p>QC/QA-HMA, 3, 70, SURFACE, 9.5 mm</p>	<p>83</p> <p>TON</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u>ATW</u> <u>11/24/12</u></p>	<p>Checked By: <u>BWS</u> <u>11/24/12</u></p>	
<p>401-07328</p>	<p>QC/QA-HMA, 3, 70, SURFACE, 9.5 mm</p>	<p>83</p> <p>TON</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u>ATW</u> <u>11/24/12</u></p>	<p>Checked By: <u>BWS</u> <u>11/24/12</u></p>	
<p>401-07328</p>	<p>QC/QA-HMA, 3, 70, SURFACE, 9.5 mm</p>	<p>83</p> <p>TON</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u>ATW</u> <u>11/24/12</u></p>	<p>Checked By: <u>BWS</u> <u>11/24/12</u></p>	
<p>401-07328</p>	<p>QC/QA-HMA, 3, 70, SURFACE, 9.5 mm</p>	<p>83</p> <p>TON</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u>ATW</u> <u>11/24/12</u></p>	<p>Checked By: <u>BWS</u> <u>11/24/12</u></p>	
<p>401-07328</p>	<p>QC/QA-HMA, 3, 70, SURFACE, 9.5 mm</p>	<p>83</p> <p>TON</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u>ATW</u> <u>11/24/12</u></p>	<p>Checked By: <u>BWS</u> <u>11/24/12</u></p>	
<p>401-07328</p>	<p>QC/QA-HMA, 3, 70, SURFACE, 9.5 mm</p>	<p>83</p> <p>TON</p>

[illegible]

<i>TOTAL =</i>		83.0
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<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/17/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
501-06266	PROFILOGRAPH, PCCP	1 LS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/17/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
501-06266	PROFILOGRAPH, PCCP	1 LS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> </u>	<u> <i>4/17/12</i> </u>	Checked By: <u> <i>ATW</i> </u>
501-06266	PROFILOGRAPH, PCCP	1 LS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> </u>	<u> <i>4/17/12</i> </u>	Checked By: <u> <i>ATW</i> </u>
501-06266	PROFILOGRAPH, PCCP	1 LS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/17/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
501-06266	PROFILOGRAPH, PCCP	1 LS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/17/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
501-06266	PROFILOGRAPH, PCCP	1 LS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/17/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
501-06266	PROFILOGRAPH, PCCP	1 LS

[illegible]

<i>SUBTOTAL (THIS PAGE) =</i>		<i>1.0</i>
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TOTAL = 1.0

<i>SUBTOTAL (THIS PAGE) =</i>		<i>49021.0</i>
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SYS

[illegible]

<i>SUBTOTAL (THIS PAGE) =</i>		<i>-4659.8</i>
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TOTAL = 44361.3

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
503-05240	D-1 CONTRACTION JOINT	22,181 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
503-05240	D-1 CONTRACTION JOINT	22,181 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
503-05240	D-1 CONTRACTION JOINT	22,181 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
503-05240	D-1 CONTRACTION JOINT	22,181 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
503-05240	D-1 CONTRACTION JOINT	22,181 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
503-05240	D-1 CONTRACTION JOINT	22,181 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
503-05240	D-1 CONTRACTION JOINT	22,181 LFT

[illegible]

SUBTOTAL (THIS PAGE) =		22181.0
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TOTAL = 22181.0

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
<i>By:</i> <u> <i>SRS</i> <i>11/24/12</i> </u>	<i>Checked By:</i> <u> <i>BWS</i> <i>11/25/12</i> </u>	
601-01522	GUARDRAIL, TRANSITION TYPE TGB	3 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>SRS</i> <i>11/24/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/25/12</i> </u>	
601-01522	GUARDRAIL, TRANSITION TYPE TGB	3 EACH

<p style="text-align: right;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> <i>SRS</i> <i>11/24/12</i> </u> Checked By: <u> <i>BWS</i> <i>11/25/12</i> </u> </p>		
601-01522	GUARDRAIL, TRANSITION TYPE TGB	3 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>SRS</i> <i>11/24/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/25/12</i> </u>	
601-01522	GUARDRAIL, TRANSITION TYPE TGB	3 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>SRS</i> <i>11/24/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/25/12</i> </u>	
601-01522	GUARDRAIL, TRANSITION TYPE TGB	3 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
<i>By:</i> <u> <i>SRS</i> <i>11/24/12</i> </u>	<i>Checked By:</i> <u> <i>BWS</i> <i>11/25/12</i> </u>	
601-01522	GUARDRAIL, TRANSITION TYPE TGB	3 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
<i>By:</i> <u> <i>SRS</i> <i>11/24/12</i> </u>	<i>Checked By:</i> <u> <i>BWS</i> <i>11/25/12</i> </u>	
601-01522	GUARDRAIL, TRANSITION TYPE TGB	3 EACH

[illegible]

					TOTAL =	3.0
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<p style="text-align: right;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> <i>SRS</i> <i>11/24/12</i> </u> Checked By: <u> <i>BWS</i> <i>11/25/12</i> </u> </p>		
601-94689	GUARDRAIL END TREATMENT, OS	3 EACH

<p align="center"><i>10-703</i></p> <p align="center"><i>SR 37 MOBILITY STUDY</i></p> <p align="center"><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>SRS</i> <i>11/24/12</i> </u></p>	<p>Checked By: <u> <i>BWS</i> <i>11/25/12</i> </u></p>	
<p>601-94689</p>	<p>GUARDRAIL END TREATMENT, OS</p>	<p align="center">3</p> <p align="center">EACH</p>

<p align="center"><i>10-703</i></p> <p align="center"><i>SR 37 MOBILITY STUDY</i></p> <p align="center"><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>SRS</i> <i>11/24/12</i> </u></p>	<p>Checked By: <u> <i>BWS</i> <i>11/25/12</i> </u></p>	
<p>601-94689</p>	<p>GUARDRAIL END TREATMENT, OS</p>	<p align="center">3</p> <p align="center">EACH</p>

<p align="center"><i>10-703</i></p> <p align="center"><i>SR 37 MOBILITY STUDY</i></p> <p align="center"><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>SRS</i> <i>11/24/12</i> </u></p>	<p>Checked By: <u> <i>BWS</i> <i>11/25/12</i> </u></p>	
<p>601-94689</p>	<p>GUARDRAIL END TREATMENT, OS</p>	<p align="center">3</p> <p align="center">EACH</p>

<p align="center"><i>10-703</i></p> <p align="center"><i>SR 37 MOBILITY STUDY</i></p> <p align="center"><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>SRS</i> <i>11/24/12</i> </u></p>	<p>Checked By: <u> <i>BWS</i> <i>11/25/12</i> </u></p>	
<p>601-94689</p>	<p>GUARDRAIL END TREATMENT, OS</p>	<p align="center">3</p> <p align="center">EACH</p>

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
<i>By:</i> <u> <i>SRS</i> <i>11/24/12</i> </u>	<i>Checked By:</i> <u> <i>BWS</i> <i>11/25/12</i> </u>	
601-94689	GUARDRAIL END TREATMENT, OS	3 EACH

<p align="center"><i>10-703</i></p> <p align="center"><i>SR 37 MOBILITY STUDY</i></p> <p align="center"><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>SRS</i> <i>11/24/12</i> </u></p>	<p>Checked By: <u> <i>BWS</i> <i>11/25/12</i> </u></p>	
<p>601-94689</p>	<p>GUARDRAIL END TREATMENT, OS</p>	<p align="center">3</p> <p align="center">EACH</p>

[illegible]

					<i>TOTAL =</i>	3.0
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<p style="text-align: right;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> <i>SRS</i> <i>11/24/12</i> </u> Checked By: <u> <i>BWS</i> <i>11/25/12</i> </u></p>		
601-99105	GUARDRAIL, W-BEAM, 6 FT 3 IN SPACING	1,668 LFT

<p style="text-align: right;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> <i>SRS</i> <i>11/24/12</i> </u> Checked By: <u> <i>BWS</i> <i>11/25/12</i> </u> </p>		
601-99105	GUARDRAIL, W-BEAM, 6 FT 3 IN SPACING	1,668 LFT

<p style="text-align: right;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> <i>SRS</i> <i>11/24/12</i> </u> Checked By: <u> <i>BWS</i> <i>11/25/12</i> </u></p>		
601-99105	GUARDRAIL, W-BEAM, 6 FT 3 IN SPACING	1,668 LFT

<p style="text-align: right;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> <i>SRS</i> <i>11/24/12</i> </u> Checked By: <u> <i>BWS</i> <i>11/25/12</i> </u></p>		
601-99105	GUARDRAIL, W-BEAM, 6 FT 3 IN SPACING	1,668 LFT

<p style="text-align: right;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> <i>SRS</i> <i>11/24/12</i> </u> Checked By: <u> <i>BWS</i> <i>11/25/12</i> </u> </p>		
601-99105	GUARDRAIL, W-BEAM, 6 FT 3 IN SPACING	1,668 LFT

<p style="text-align: right;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> <i>SRS</i> <i>11/24/12</i> </u> Checked By: <u> <i>BWS</i> <i>11/25/12</i> </u></p>		
601-99105	GUARDRAIL, W-BEAM, 6 FT 3 IN SPACING	1,668 LFT

<p style="text-align: right;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> <i>SRS</i> <i>11/24/12</i> </u> Checked By: <u> <i>BWS</i> <i>11/25/12</i> </u> </p>		
601-99105	GUARDRAIL, W-BEAM, 6 FT 3 IN SPACING	1,668 LFT

[illegible]

						<i>TOTAL =</i>	<i>1668.0</i>
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<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/20/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
602-06729	BARRIER DELINEATOR	51 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/20/12</i> </u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i> </u>	
602-06729	BARRIER DELINEATOR	51 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/20/12</i> </u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i> </u>	
602-06729	BARRIER DELINEATOR	51 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/20/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
602-06729	BARRIER DELINEATOR	51 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/20/12</i> </u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i> </u>	
602-06729	BARRIER DELINEATOR	51 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/20/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
602-06729	BARRIER DELINEATOR	51 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/20/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
602-06729	BARRIER DELINEATOR	51 EACH

[illegible]

<i>SUBTOTAL (THIS PAGE) =</i>		<i>51.0</i>
<i>TOTAL =</i>		<i>51.0</i>

TOTAL = 51.0

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/13/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
602-08603	CONCRETE BARRIER, 45 IN	1,991 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/13/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
602-08603	CONCRETE BARRIER, 45 IN	1,991 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/13/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
602-08603	CONCRETE BARRIER, 45 IN	1,991 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/13/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
602-08603	CONCRETE BARRIER, 45 IN	1,991 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/13/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
602-08603	CONCRETE BARRIER, 45 IN	1,991 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/13/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
602-08603	CONCRETE BARRIER, 45 IN	1,991 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/13/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/24/12</i></u>	
602-08603	CONCRETE BARRIER, 45 IN	1,991 LFT

[illegible]

<i>SUBTOTAL (THIS PAGE) =</i>		<i>1990.9</i>
<i>TOTAL =</i>		<i>1990.9</i>

TOTAL = *1990.9*

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>11/20/12</i> </u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u>	
603-06040	FENCE, FARM FIELD, 47 IN	5,418 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>11/20/12</i> </u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u>	
603-06040	FENCE, FARM FIELD, 47 IN	5,418 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>11/20/12</i> </u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u>	
603-06040	FENCE, FARM FIELD, 47 IN	5,418 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>11/20/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
603-06040	FENCE, FARM FIELD, 47 IN	5,418 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>11/20/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
603-06040	FENCE, FARM FIELD, 47 IN	5,418 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>11/20/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
603-06040	FENCE, FARM FIELD, 47 IN	5,418 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>11/20/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
603-06040	FENCE, FARM FIELD, 47 IN	5,418 LFT

[illegible]

					<i>SUBTOTAL (THIS PAGE) =</i>		<i>5417.8</i>
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TOTAL = 5417.8

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>JPS</i> <i>11/21/12</i> </u></p>	<p>Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u></p>	<p>604-07569</p> <p style="text-align: center;">PAVERS</p> <p style="text-align: right;">491</p> <p style="text-align: right;">SYS</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>JPS</i> <i>11/21/12</i> </u></p>	<p>Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u></p>	<p>604-07569</p> <p style="text-align: center;">PAVERS</p> <p style="text-align: right;">491</p> <p style="text-align: right;">SYS</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>JPS</i> <i>11/21/12</i> </u></p>	<p>Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u></p>	<p>604-07569</p> <p style="text-align: center;">PAVERS</p> <p style="text-align: right;">491</p> <p style="text-align: right;">SYS</p>

<p>10-703</p> <p>SR 37 MOBILITY STUDY</p> <p>Town and Country Boulevard</p>		
<p>By: <u> JPS 11/21/12 </u></p>	<p>Checked By: <u> BWS 11/24/12 </u></p>	
<p>604-07569</p>	<p>PAVERS</p>	<p>491</p> <p>SYS</p>

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>JPS</i> <i>11/21/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
604-07569	PAVERS	491
		SYS

<p>10-703</p> <p>SR 37 MOBILITY STUDY</p> <p>Town and Country Boulevard</p>		
<p>By: <u> JPS 11/21/12 </u></p>	<p>Checked By: <u> BWS 11/24/12 </u></p>	
<p>604-07569</p>	<p>PAVERS</p>	<p>491</p> <p>SYS</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>JPS</i> <i>11/21/12</i> </u></p>	<p>Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u></p>	<p>604-07569</p> <p style="text-align: center;">PAVERS</p> <p style="text-align: right;">491</p> <p style="text-align: right;">SYS</p>

[illegible]

<i>SUBTOTAL (THIS PAGE) =</i>		<i>490.8</i>
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TOTAL = 490.8

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>JPS</i> <i>11/21/12</i> </u></p>	<p>Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u></p>	
<p>605-06120</p>	<p>CURB, CONCRETE</p>	<p>619</p> <p>LFT</p>

<p>10-703</p> <p>SR 37 MOBILITY STUDY</p> <p>Town and Country Boulevard</p>		
<p>By: <u> JPS 11/21/12 </u></p>	<p>Checked By: <u> BWS 11/24/12 </u></p>	
<p>605-06120</p>	<p>CURB, CONCRETE</p>	<p>619</p> <p>LFT</p>

<p>10-703</p> <p>SR 37 MOBILITY STUDY</p> <p>Town and Country Boulevard</p>		
<p>By: <u> JPS 11/21/12 </u></p>	<p>Checked By: <u> BWS 11/24/12 </u></p>	
<p>605-06120</p>	<p>CURB, CONCRETE</p>	<p>619</p> <p>LFT</p>

<p>10-703</p> <p>SR 37 MOBILITY STUDY</p> <p>Town and Country Boulevard</p>		
<p>By: <u> JPS 11/21/12 </u></p>	<p>Checked By: <u> BWS 11/24/12 </u></p>	
<p>605-06120</p>	<p>CURB, CONCRETE</p>	<p>619</p> <p>LFT</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>JPS</i> <i>11/21/12</i> </u></p>	<p>Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u></p>	
<p>605-06120</p>	<p>CURB, CONCRETE</p>	<p>619</p> <p>LFT</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>JPS</i> <i>11/21/12</i> </u></p>	<p>Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u></p>	
<p>605-06120</p>	<p>CURB, CONCRETE</p>	<p>619</p> <p>LFT</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>JPS</i> <i>11/21/12</i> </u></p>	<p>Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u></p>	
<p>605-06120</p>	<p>CURB, CONCRETE</p>	<p>619</p> <p>LFT</p>

[illegible]

SUBTOTAL (THIS PAGE) =		619.0
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TOTAL = 619.0

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/12/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/25/12</i></u>	
605-06140	CURB AND GUTTER, CONCRETE	1,398 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/12/12</i> </u>	Checked By: <u> <i>ATW</i> <i>11/25/12</i> </u>	
605-06140	CURB AND GUTTER, CONCRETE	1,398 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/12/12</i> </u>	Checked By: <u> <i>ATW</i> <i>11/25/12</i> </u>	
605-06140	CURB AND GUTTER, CONCRETE	1,398 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/12/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/25/12</i></u>	
605-06140	CURB AND GUTTER, CONCRETE	1,398 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/12/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/25/12</i></u>	
605-06140	CURB AND GUTTER, CONCRETE	1,398 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/12/12</i> </u>	Checked By: <u> <i>ATW</i> <i>11/25/12</i> </u>	
605-06140	CURB AND GUTTER, CONCRETE	1,398 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/12/12</i> </u>	Checked By: <u> <i>ATW</i> <i>11/25/12</i> </u>	
605-06140	CURB AND GUTTER, CONCRETE	1,398 LFT

[illegible]

SUBTOTAL (THIS PAGE) =		1398.0
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TOTAL = 1398.0

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: JPS 11/21/12

Checked By: BWS 11/24/12

605-06145

CURB AND GUTTER, B, CONCRETE

846
LFT

[illegible]

<i>SUBTOTAL (THIS PAGE) =</i>		845.7
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TOTAL = 845.7

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/13/12</i></u>	Checked By: <u> <i>JPS</i> <i>11/21/12</i></u>	
605-06255	CENTER CURB, D, CONCRETE	422 SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> </u>	<u> <i>4/13/12</i> </u>	Checked By: <u> <i>JPS</i> </u>
605-06255	CENTER CURB, D, CONCRETE	422 SYS

<p>10-703</p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS 4/13/12 </u></p>	<p>Checked By: <u> JPS 11/21/12 </u></p>	
<p>605-06255</p>	<p>CENTER CURB, D, CONCRETE</p>	<p>422</p> <p>SYS</p>

<p>10-703</p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS 4/13/12 </u></p>	<p>Checked By: <u> JPS 11/21/12 </u></p>	
<p>605-06255</p>	<p>CENTER CURB, D, CONCRETE</p>	<p>422</p> <p>SYS</p>

<p>10-703</p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS 4/13/12 </u></p>	<p>Checked By: <u> JPS 11/21/12 </u></p>	
<p>605-06255</p>	<p>CENTER CURB, D, CONCRETE</p>	<p>422</p> <p>SYS</p>

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/13/12</i></u>	Checked By: <u> <i>JPS</i> <i>11/21/12</i></u>	
605-06255	CENTER CURB, D, CONCRETE	422 SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/13/12</i></u>	Checked By: <u> <i>JPS</i> <i>11/21/12</i></u>	
605-06255	CENTER CURB, D, CONCRETE	422 SYS

[illegible]

		<i>SUBTOTAL (THIS PAGE) =</i>		<i>421.4</i>
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TOTAL = 421.4

10-703

**SR 37 MOBILITY STUDY
Town and Country Boulevard**

By: BWS 4/17/12Checked By: BWC 11/24/12

615-06510

MONUMENT, C

5
EACH

Alignment	Station	Description	Inside Pavement?			Each
Line "S-2-A"						
	10+00.00	Begin Project		Yes		
	13+53.23	PI		Yes		
	13+62.18	PI		Yes		
	17+76.64	End Project		Yes		
Line "A"						
	238+33.71	Begin Project		No		1
	246+11.03	Between Begin and PT		Yes		
	253+88.35	PT		Yes		
	261+15.49	Between PT and End Project		Yes		
	268+42.63	End Project		Yes		
SW Ramp						
	00+00.00	Begin Project		Yes		
	02+96.19	PC		Yes		
	04+05.10	PI		No		1
	04+46.88	End Project		Yes		
NW Ramp						
	00+43.88	Begin Project		Yes		
	01+09.20	PI		No		1
	02+43.77	PT		Yes		
	06+48.63	End Project		Yes		
NE Ramp						
	00+00.00	Begin Project		Yes		
	04+54.18	PC		Yes		
	05+63.22	PI		No		1
	06+04.87	End Project		Yes		
SE Ramp						
	00+61.18	Begin Project		Yes		
	01+41.41	PI		No		1
	02+61.44	PT		Yes		
	06+64.43	End Project		Yes		

SUBTOTAL (THIS PAGE) = 5.0

TOTAL = 5.0

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: BWS 4/17/12

Checked By: BWC 11/24/12

615-06515

MONUMENT, D

20
EACH

[illegible]

<i>SUBTOTAL (THIS PAGE) =</i>		<i>20.0</i>
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TOTAL = 20.0

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
616-02320	GEOTEXTILES	446 SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> </u>	<u> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> </u>
616-02320	GEOTEXTILES	446 SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
616-02320	GEOTEXTILES	446 SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> </u>	<u> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> </u>
616-02320	GEOTEXTILES	446 SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> </u>	<u> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> </u>
616-02320	GEOTEXTILES	446 SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> </u>	<u> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> </u>
616-02320	GEOTEXTILES	446 SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> </u>	<u> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> </u>
616-02320	GEOTEXTILES	446 SYS

[illegible]

	<i>TOTAL =</i>	445.3
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<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/11/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
616-06405	RIPRAP, REVETMENT	251 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
<i>By:</i> <u> <i>DJZ</i> <i>4/11/12</i> </u>	<i>Checked By:</i> <u> <i>BWS</i> <i>11/24/12</i> </u>	
616-06405	RIPRAP, REVETMENT	251 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/11/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
616-06405	RIPRAP, REVETMENT	251 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/11/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
616-06405	RIPRAP, REVETMENT	251 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/11/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
616-06405	RIPRAP, REVETMENT	251 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/11/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
616-06405	RIPRAP, REVETMENT	251 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/11/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
616-06405	RIPRAP, REVETMENT	251 TON

[illegible]

	<i>TOTAL =</i>	250.7
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10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: DJZ 4/11/12

Checked By: BWS 11/24/12

621-01004

MOBILIZATION AND DEMOBILIZATION FOR SEEDING

4
EACH

[illegible]

TOTAL = 4.0

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/25/12</i></u>	
621-06545	FERTILIZER	4 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/25/12</i> </u>	
621-06545	FERTILIZER	4 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/25/12</i> </u>	
621-06545	FERTILIZER	4 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/25/12</i></u>	
621-06545	FERTILIZER	4 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/25/12</i> </u>	
621-06545	FERTILIZER	4 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/25/12</i></u>	
621-06545	FERTILIZER	4 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/25/12</i></u>	
621-06545	FERTILIZER	4 TON

[illegible]

		<i>TOTAL =</i>		<i>3.0</i>
--	--	----------------	--	------------

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>
--	---

621-06554	SEED MIXTURE, U	864 LBS
------------------	------------------------	----------------

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>
--	---

621-06554	SEED MIXTURE, U	864 LBS
------------------	------------------------	--------------------

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>
--	---

621-06554	SEED MIXTURE, U	864 LBS
------------------	------------------------	--------------------

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06554	SEED MIXTURE, U	864 LBS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06554	SEED MIXTURE, U	864 LBS

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: <u> <i>DJZ</i> <i>4/25/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>
--	---

621-06554	SEED MIXTURE, U	864 LBS
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<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
<i>By:</i> <u> <i>DJZ</i> <i>4/25/12</i> </u>	<i>Checked By:</i> <u> <i>BWS</i> <i>11/24/12</i> </u>	
621-06554	SEED MIXTURE, U	864 LBS

[illegible]

	<i>TOTAL =</i>	863.1
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<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06557	SEED MIXTURE, T	381 LBS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06557	SEED MIXTURE, T	381 LBS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06557	SEED MIXTURE, T	381 LBS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06557	SEED MIXTURE, T	381 LBS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06557	SEED MIXTURE, T	381 LBS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06557	SEED MIXTURE, T	381 LBS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06557	SEED MIXTURE, T	381 LBS

[illegible]

	<i>TOTAL =</i>	380.8
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<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06565	MULCHING MATERIAL	16 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06565	MULCHING MATERIAL	16 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06565	MULCHING MATERIAL	16 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06565	MULCHING MATERIAL	16 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
621-06565	MULCHING MATERIAL	16 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06565	MULCHING MATERIAL	16 TON

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06565	MULCHING MATERIAL	16 TON

[illegible]

TOTAL = 15.2

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: DJZ 4/25/12

Checked By: BWS 11/24/12

621-06567

WATER

12
kGAL

[illegible]

<i>TOTAL =</i>	<i>11.4</i>
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<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06574	SODDING	2,857
		SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06574	SODDING	2,857
		SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06574	SODDING	2,857
		SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06574	SODDING	2,857
		SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06574	SODDING	2,857
		SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06574	SODDING	2,857
		SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/25/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
621-06574	SODDING	2,857
		SYS

[illegible]

	<i>TOTAL =</i>	2856.0
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10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: DJZ 4/17/12

Checked By: _____

628-08520

CELLULAR TELEPHONE/RADIO

2
EACH

[illegible]

SUBTOTAL (THIS PAGE) = 2.0

TOTAL = 2.0

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: DJZ 4/17/12

Checked By: BWS 11/24/12

628-08521

CELLULAR TELEPHONE/RADIO SERVICE

36
MOS

[illegible]

<i>SUBTOTAL (THIS PAGE) =</i>		<i>36.0</i>
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TOTAL = 36.0

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> DJZ 4/17/12 </u>	Checked By: <u> BWS 11/24/12 </u>	
628-09403	FIELD OFFICE, C	18 MOS

<p>10-703</p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> DJZ 4/17/12 </u></p>	<p>Checked By: <u> BWS 11/24/12 </u></p>	
<p>628-09403</p>	<p>FIELD OFFICE, C</p>	<p>18</p> <p>MOS</p>

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> </u>	<u> <i>4/17/12</i> </u>	Checked By: <u> <i>BWS</i> </u>
628-09403	FIELD OFFICE, C	18 MOS

<p>10-703</p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> DJZ 4/17/12 </u></p>	<p>Checked By: <u> BWS 11/24/12 </u></p>	
<p>628-09403</p>	<p>FIELD OFFICE, C</p>	
<p>18</p> <p>MOS</p>		

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>4/17/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
628-09403	FIELD OFFICE, C	18 MOS

<p>10-703</p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> DJZ 4/17/12 </u></p>	<p>Checked By: <u> BWS 11/24/12 </u></p>	
<p>628-09403</p>	<p>FIELD OFFICE, C</p>	<p>18</p> <p>MOS</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> DJZ 4/17/12 </u></p>	<p>Checked By: <u> BWS 11/24/12 </u></p>	
<p>628-09403</p>	<p>FIELD OFFICE, C</p>	<p>18</p> <p>MOS</p>

[illegible]

<i>SUBTOTAL (THIS PAGE) =</i>		<i>18.0</i>
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TOTAL = 18.0

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: BWS 4/23/12

Checked By: BWC 11/24/12

701-90386

TEMPORARY SHEET PILING

1
LS

[illegible]

<i>TOTAL</i> =	<i>1.0</i>
-----------------------	-------------------

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: srs 11/19/12

Checked By: ATW 11/25/12

706-08496

REINFORCED CONCRETE MOMENT SLAB, 12 IN

**2,982
SYS**

[illegible]

SUBTOTAL (THIS PAGE) =		2981.7
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TOTAL = 2981.7

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/25/12</i></u>	
706-09545	COARSE AGGREGATE, NO 8	746 CYS

10-703		
SR 37 MOBILITY STUDY Town and Country Boulevard		
By: <u> srs 11/19/12 </u>	Checked By: <u> ATW 11/25/12 </u>	
706-09545	COARSE AGGREGATE, NO 8	746 CYS

10-703		
SR 37 MOBILITY STUDY Town and Country Boulevard		
By: <u> srs 11/19/12 </u>	Checked By: <u> ATW 11/25/12 </u>	
706-09545	COARSE AGGREGATE, NO 8	746 CYS

10-703		
SR 37 MOBILITY STUDY Town and Country Boulevard		
By: <u> srs 11/19/12 </u>	Checked By: <u> ATW 11/25/12 </u>	
706-09545	COARSE AGGREGATE, NO 8	746 CYS

10-703		
SR 37 MOBILITY STUDY Town and Country Boulevard		
By: <u> srs 11/19/12 </u>	Checked By: <u> ATW 11/25/12 </u>	
706-09545	COARSE AGGREGATE, NO 8	746 CYS

10-703		
SR 37 MOBILITY STUDY Town and Country Boulevard		
By: <u> srs 11/19/12 </u>	Checked By: <u> ATW 11/25/12 </u>	
706-09545	COARSE AGGREGATE, NO 8	746 CYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i></u>	Checked By: <u> <i>ATW</i> <i>11/25/12</i></u>	
706-09545	COARSE AGGREGATE, NO 8	746 CYS

[illegible]

<i>SUBTOTAL (THIS PAGE) =</i>		<i>745.4</i>
<i>TOTAL =</i>		<i>745.4</i>

TOTAL = 745.4

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: srs 11/24/12

Checked By: BWS 11/25/12

706-09959

RAILING, CONCRETE, FT

**6,706
LFT**

[illegible]

<i>SUBTOTAL (THIS PAGE) =</i>		<i>6705.4</i>
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TOTAL = 6705.4

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: srs 11/19/12

Checked By: BWS 11/24/12

715-05048

PIPE, TYPE 4 CIRCULAR 6 IN

12,099
LFT

[illegible]

TOTAL =	12098.0
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TOTAL = 12098.0

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: srs 11/19/12

Checked By: BWS 11/24/12

715-05053

PIPE, UNDERDRAIN, OUTLET 6 IN

270
LFT

[illegible]

<i>TOTAL</i> =	270.0
-----------------------	--------------

TOTAL = 270.0

10-703

**SR 37 MOBILITY STUDY
Town and Country Boulevard**

By: BWS 11/19/12Checked By: BWC 11/24/12**715-05149****PIPE, TYPE 2 CIRCULAR 12 IN****5,593
LFT**

<i>Station</i>						<i>Lft</i>
<i>Use 300' inlet spacing</i>						
<i>Line "A"</i>	<i>Median Inlets</i>					
						80
						92
						38
						296
						296
						296
						246
						246
						296
						296
						296
						38
<i>Line "A"</i>	<i>Outside Wall Inlets</i>					
	<i>Rt</i>					54
	<i>Lt</i>					54
	<i>Rt</i>					66
	<i>Lt</i>					54
	<i>Rt</i>					38
	<i>Lt</i>					38
	<i>Rt</i>					38
	<i>Lt</i>					38
	<i>Rt</i>					66
	<i>Lt</i>					66
	<i>Rt</i>					54
	<i>Lt</i>					66
<i>Line "S-2-A"</i>						
50+50.00						157
51+50.00						182
52+70.00						151
53+50.00						173
54+50.00						556
58+50.00						651
59+50.00						172
60+50.00						167
61+50.00						167
62+50.00						67

SUBTOTAL (THIS PAGE) = 5592.4

TOTAL = 5592.4

<p style="text-align: right;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> <i>BWS</i> <i>11/19/12</i> </u> Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u></p>		
715-09064	VIDEO INSPECTION FOR PIPE	5,593 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>11/19/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
715-09064	VIDEO INSPECTION FOR PIPE	5,593 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>11/19/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
715-09064	VIDEO INSPECTION FOR PIPE	5,593 LFT

<p style="text-align: right;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> <i>BWS</i> <i>11/19/12</i> </u> Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u></p>		
715-09064	VIDEO INSPECTION FOR PIPE	5,593 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>11/19/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
715-09064	VIDEO INSPECTION FOR PIPE	5,593 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>11/19/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
715-09064	VIDEO INSPECTION FOR PIPE	5,593 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>11/19/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
715-09064	VIDEO INSPECTION FOR PIPE	5,593 LFT

[illegible]

<i>SUBTOTAL (THIS PAGE) =</i>		<i>5593.0</i>
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TOTAL = 5593.0

<p align="right"><i>10-703</i></p> <p align="center"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> <i>srs</i> <i>11/19/12</i> </u> Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u></p>		
718-06528	OUTLET PROTECTOR, 1	10 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
718-06528	OUTLET PROTECTOR, 1	10 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
718-06528	OUTLET PROTECTOR, 1	10 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
718-06528	OUTLET PROTECTOR, 1	10 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
718-06528	OUTLET PROTECTOR, 1	10 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
718-06528	OUTLET PROTECTOR, 1	10 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> 11/19/12 </u>	Checked By: <u> <i>BWS</i> 11/24/12 </u>	
718-06528	OUTLET PROTECTOR, 1	10 EACH

[illegible]

		<i>TOTAL =</i>	<i>10.0</i>
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TOTAL = 10.0

<p style="text-align: right;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> <i>srs</i> <i>11/19/12</i> </u> Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u></p>		
718-06532	VIDEO INSPECTION FOR UNDERDRAINS	3,000 LFT

<p style="text-align: right;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> <i>srs</i> <i>11/19/12</i> </u> Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u></p>		
718-06532	VIDEO INSPECTION FOR UNDERDRAINS	
		3,000 LFT

<p style="text-align: right;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> <i>srs</i> <i>11/19/12</i> </u> Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u></p>		
718-06532	VIDEO INSPECTION FOR UNDERDRAINS	3,000 LFT

<p style="text-align: right;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> <i>srs</i> <i>11/19/12</i> </u> Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u></p>		
718-06532	VIDEO INSPECTION FOR UNDERDRAINS	3,000 LFT

<p style="text-align: right;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> <i>srs</i> <i>11/19/12</i> </u> Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u></p>		
718-06532	VIDEO INSPECTION FOR UNDERDRAINS	
		3,000 LFT

<p style="text-align: right;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> <i>srs</i> <i>11/19/12</i> </u> Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u></p>		
718-06532	VIDEO INSPECTION FOR UNDERDRAINS	3,000 LFT

<p style="text-align: right;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> <i>srs</i> <i>11/19/12</i> </u> Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u></p>		
718-06532	VIDEO INSPECTION FOR UNDERDRAINS	
		3,000 LFT

[illegible]

<i>TOTAL =</i>	<i>3000.0</i>
<i>TOTAL =</i>	<i>3000.0</i>

<i>10-703</i>		
<p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
718-52610	AGGREGATE FOR UNDERDRAINS	1,089 CYS

<i>10-703</i>		
<p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
718-52610	AGGREGATE FOR UNDERDRAINS	1,089 CYS

<i>10-703</i>		
<p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
718-52610	AGGREGATE FOR UNDERDRAINS	1,089 CYS

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>
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718-52610	AGGREGATE FOR UNDERDRAINS	1,089 CYS
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<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> </u>	Checked By: <u> <i>BWS</i> </u>	<u> <i>11/19/12</i> </u>
718-52610	AGGREGATE FOR UNDERDRAINS	1,089 CYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
718-52610	AGGREGATE FOR UNDERDRAINS	1,089 CYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
718-52610	AGGREGATE FOR UNDERDRAINS	1,089 CYS

[illegible]

TOTAL = 1088.9

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
718-99153	GEOTEXTILES FOR UNDERDRAIN	8,291 SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
718-99153	GEOTEXTILES FOR UNDERDRAIN	8,291 SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
718-99153	GEOTEXTILES FOR UNDERDRAIN	8,291 SYS

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>
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718-99153	GEOTEXTILES FOR UNDERDRAIN	8,291 SYS
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<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i></u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i></u>	
718-99153	GEOTEXTILES FOR UNDERDRAIN	8,291 SYS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> </u>	Checked By: <u> <i>BWS</i> </u>	
718-99153	GEOTEXTILES FOR UNDERDRAIN	8,291 SYS

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>
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718-99153	GEOTEXTILES FOR UNDERDRAIN	8,291 SYS
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[illegible]

	<i>TOTAL =</i>	8290.1
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TOTAL = *8290.1*

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: BWS 5/11/12

Checked By: BWC 11/24/12

720-07300

INLET, TYPE H, WITH SLOTTED DRAIN

**12
EACH**

[illegible]

SUBTOTAL (THIS PAGE) =		12.0
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TOTAL = 12.0

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: BWS 5/11/12

Checked By: BWC 11/24/12

720-07302

INLET, TYPE HA, WITH SLOTTED DRAIN

**12
EACH**

[illegible]

<i>SUBTOTAL (THIS PAGE) =</i>		<i>12.0</i>
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TOTAL = 12.0

<p>10-703</p> <p>SR 37 MOBILITY STUDY</p> <p>Town and Country Boulevard</p>		
<p>By: <u> BWS 11/19/12 </u></p>	<p>Checked By: <u> BWC 11/24/12 </u></p>	
<p>720-45410</p>	<p>MANHOLE, C4</p>	<p>12</p> <p>EACH</p>

<p>10-703</p> <p>SR 37 MOBILITY STUDY</p> <p>Town and Country Boulevard</p>		
<p>By: <u> BWS 11/19/12 </u></p>	<p>Checked By: <u> BWC 11/24/12 </u></p>	
<p>720-45410</p>	<p>MANHOLE, C4</p>	<p>12</p> <p>EACH</p>

<p>10-703</p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS 11/19/12 </u></p>	<p>Checked By: <u> BWC 11/24/12 </u></p>	
<p>720-45410</p>	<p>MANHOLE, C4</p>	<p>12</p> <p>EACH</p>

<p>10-703</p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS 11/19/12 </u></p>	<p>Checked By: <u> BWC 11/24/12 </u></p>	
<p>720-45410</p>	<p>MANHOLE, C4</p>	<p>12</p> <p>EACH</p>

<p>10-703</p> <p>SR 37 MOBILITY STUDY</p> <p>Town and Country Boulevard</p>		
<p>By: <u> BWS 11/19/12 </u></p>	<p>Checked By: <u> BWC 11/24/12 </u></p>	
<p>720-45410</p>	<p>MANHOLE, C4</p>	<p>12</p> <p>EACH</p>

<p>10-703</p> <p>SR 37 MOBILITY STUDY</p> <p>Town and Country Boulevard</p>		
<p>By: <u> BWS 11/19/12 </u></p>	<p>Checked By: <u> BWC 11/24/12 </u></p>	
<p>720-45410</p>	<p>MANHOLE, C4</p>	<p>12</p> <p>EACH</p>

<p>10-703</p> <p>SR 37 MOBILITY STUDY</p> <p>Town and Country Boulevard</p>		
<p>By: <u> BWS 11/19/12 </u></p>	<p>Checked By: <u> BWC 11/24/12 </u></p>	
<p>720-45410</p>	<p>MANHOLE, C4</p>	<p>12</p> <p>EACH</p>

[illegible]

<i>SUBTOTAL (THIS PAGE) =</i>		<i>12.0</i>
--------------------------------------	--	--------------------

TOTAL = *12.0*

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>11/19/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
720-98174	INLET, B15	12 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>11/19/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
720-98174	INLET, B15	12 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>11/19/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
720-98174	INLET, B15	12 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>11/19/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
720-98174	INLET, B15	12 EACH

<p>10-703</p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS 11/19/12 </u></p>	<p>Checked By: <u> BWC 11/24/12 </u></p>	
<p>720-98174</p>	<p>INLET, B15</p>	<p>12</p> <p>EACH</p>

<p>10-703</p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS 11/19/12 </u></p>	<p>Checked By: <u> BWC 11/24/12 </u></p>	
<p>720-98174</p>	<p>INLET, B15</p>	<p>12</p> <p>EACH</p>

<p>10-703</p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS 11/19/12 </u></p>	<p>Checked By: <u> BWC 11/24/12 </u></p>	
<p>720-98174</p>	<p>INLET, B15</p>	<p>12</p> <p>EACH</p>

[illegible]

<i>SUBTOTAL (THIS PAGE) =</i>		<i>12.0</i>
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TOTAL = *12.0*

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: BWS 11/19/12

Checked By: BWC 11/24/12

720-98555

INLET, C15

**12
EACH**

[illegible]

<i>SUBTOTAL (THIS PAGE) =</i>		<i>12.0</i>
--------------------------------------	--	--------------------

TOTAL = 12.0

10-703

**SR 37 MOBILITY STUDY
Town and Country Boulevard**

By: SRS 11/24/12Checked By: srp 11/26/12**731-93945****FACE PANELS, CONCRETE****58,984
SFT**

Segment	Length	Begin Height	End Height			
	(ft)	(ft)	(ft)			
Since the wall is curved and extends between two alignments (mainline and ramp), all lengths measured in AutoCAD for better accuracy. Segments measured in the direction of travel. Not every wall has all 3 segments. Assumptions made on lengths depending on what the wall looks like. * = measured directly in AutoCAD.						
Segment 1 = Transition from 4 ft to 7 ft						
Segment 2 = 7 ft (around curve)						
Segment 3 = Transition from 7 ft to 4 ft						
NE Wall	435.44					
Segment 1	108.86	4	11			816
Segment 2	217.72	11	11			2395
Segment 3	108.86	11	4			816
SE Wall	321.35					
Segment 1	--	--	--			
Segment 2	148.80	11	11			1637
Segment 3	--	--	--			
SW Wall	389.48					
Segment 1	129.83	4	11			974
Segment 2	259.65	11	11			2856
Segment 3	--	--	--			
NW Wall	519.81					
Segment 1	--	--	--			
Segment 2	228.27	11	11			2511
Segment 3	291.54	11	4			2187
INSIDE WALL AREAS COPIED FROM STRUCTURE BACKFILL AREAS (211-09226)						
NB Wall						
250+38.72	256+31.81	4	27			9187
256+31.81	257+81.13	27	27			4032
257+81.13	263+73.31	27	4			9173
SB Wall						
250+46.46	256+38.65	4	27			9179
256+38.65	257+88.23	27	27			4039
257+88.23	263+81.00	27	4			9182

SUBTOTAL (THIS PAGE) = 58983.1

TOTAL = 58983.1

10-703

**SR 37 MOBILITY STUDY
Town and Country Boulevard**

By: SRS 11/24/12Checked By: srp 11/26/12**731-93946****WALL ERECTION****58,984
SFT**

Segment	Length	Begin Height	End Height			
	(ft)	(ft)	(ft)			
<i>Since the wall is curved and extends between two alignments (mainline and ramp), all lengths measured in AutoCAD for better accuracy. Segments measured in the direction of travel. Not every wall has all 3 segments. Assumptions made on lengths depending on what the wall looks like. * = measured directly in AutoCAD.</i>						
<i>Segment 1 = Transition from 4 ft to 7 ft</i>						
<i>Segment 2 = 7 ft (around curve)</i>						
<i>Segment 3 = Transition from 7 ft to 4 ft</i>						
NE Wall	435.44					
<i>Segment 1</i>	<i>108.86</i>	<i>4</i>	<i>11</i>			<i>816</i>
<i>Segment 2</i>	<i>217.72</i>	<i>11</i>	<i>11</i>			<i>2395</i>
<i>Segment 3</i>	<i>108.86</i>	<i>11</i>	<i>4</i>			<i>816</i>
SE Wall	321.35					
<i>Segment 1</i>	<i>--</i>	<i>--</i>	<i>--</i>			
<i>Segment 2</i>	<i>148.80</i>	<i>11</i>	<i>11</i>			<i>1637</i>
<i>Segment 3</i>	<i>--</i>	<i>--</i>	<i>--</i>			
SW Wall	389.48					
<i>Segment 1</i>	<i>129.83</i>	<i>4</i>	<i>11</i>			<i>974</i>
<i>Segment 2</i>	<i>259.65</i>	<i>11</i>	<i>11</i>			<i>2856</i>
<i>Segment 3</i>	<i>--</i>	<i>--</i>	<i>--</i>			
NW Wall	519.81					
<i>Segment 1</i>	<i>--</i>	<i>--</i>	<i>--</i>			
<i>Segment 2</i>	<i>228.27</i>	<i>11</i>	<i>11</i>			<i>2511</i>
<i>Segment 3</i>	<i>291.54</i>	<i>11</i>	<i>4</i>			<i>2187</i>
INSIDE WALL AREAS COPIED FROM STRUCTURE BACKFILL AREAS (211-09226)						
NB Wall						
<i>250+38.72</i>	<i>256+31.81</i>	<i>4</i>	<i>27</i>			<i>9187</i>
<i>256+31.81</i>	<i>257+81.13</i>	<i>27</i>	<i>27</i>			<i>4032</i>
<i>257+81.13</i>	<i>263+73.31</i>	<i>27</i>	<i>4</i>			<i>9173</i>
SB Wall						
<i>250+46.46</i>	<i>256+38.65</i>	<i>4</i>	<i>27</i>			<i>9179</i>
<i>256+38.65</i>	<i>257+88.23</i>	<i>27</i>	<i>27</i>			<i>4039</i>
<i>257+88.23</i>	<i>263+81.00</i>	<i>27</i>	<i>4</i>			<i>9182</i>

SUBTOTAL (THIS PAGE) = 58983.1

TOTAL = 58983.1

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: SRS 11/24/12

Checked By: srp 11/26/12

731-93947

LEVELING PAD, CONCRETE

**4,336
LFT**

[illegible]

<i>SUBTOTAL (THIS PAGE) =</i>		4335.2
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TOTAL = **4335.2**

<p>10-703</p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS 4/24/12 </u></p>	<p>Checked By: <u> BWC 11/24/12 </u></p>	
<p>801-01093</p>	<p>TEMPORARY WORKSITE SPEED LIMIT SIGN</p> <p>ASSEMBLY</p>	<p>4</p> <p>EACH</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS 4/24/12 </u></p>	<p>Checked By: <u> BWC 11/24/12 </u></p>	
<p>801-01093</p>	<p>TEMPORARY WORKSITE SPEED LIMIT SIGN</p> <p>ASSEMBLY</p>	<p>4</p> <p>EACH</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS 4/24/12 </u></p>	<p>Checked By: <u> BWC 11/24/12 </u></p>	
<p>801-01093</p>	<p>TEMPORARY WORKSITE SPEED LIMIT SIGN</p> <p>ASSEMBLY</p>	<p>4</p> <p>EACH</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS 4/24/12 </u></p>	<p>Checked By: <u> BWC 11/24/12 </u></p>	
<p>801-01093</p>	<p>TEMPORARY WORKSITE SPEED LIMIT SIGN</p> <p>ASSEMBLY</p>	<p>4</p> <p>EACH</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS 4/24/12 </u></p>	<p>Checked By: <u> BWC 11/24/12 </u></p>	
<p>801-01093</p>	<p>TEMPORARY WORKSITE SPEED LIMIT SIGN</p> <p>ASSEMBLY</p>	<p>4</p> <p>EACH</p>

[illegible]

<i>SUBTOTAL (THIS PAGE) =</i>		<i>4.0</i>
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TOTAL = **4.0**

10-703		
SR 37 MOBILITY STUDY Town and Country Boulevard		
By: <u> BWS 4/24/12 </u>	Checked By: <u> BWC 11/24/12 </u>	
801-03290	CONSTRUCTION SIGN, C	2 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/24/12</i> </u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u>	
801-03290	CONSTRUCTION SIGN, C	2 EACH

10-703		
SR 37 MOBILITY STUDY Town and Country Boulevard		
By: <u> BWS 4/24/12 </u>	Checked By: <u> BWC 11/24/12 </u>	
801-03290	CONSTRUCTION SIGN, C	2 EACH

10-703		
SR 37 MOBILITY STUDY Town and Country Boulevard		
By: <u> BWS 4/24/12 </u>	Checked By: <u> BWC 11/24/12 </u>	
801-03290	CONSTRUCTION SIGN, C	2 EACH

10-703		
SR 37 MOBILITY STUDY Town and Country Boulevard		
By: <u> BWS 4/24/12 </u>	Checked By: <u> BWC 11/24/12 </u>	
801-03290	CONSTRUCTION SIGN, C	2 EACH

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>BWS</i> <i>4/24/12</i> </u></p>	<p>Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u></p>	
<p>801-03290</p>	<p>CONSTRUCTION SIGN, C</p>	<p>2</p> <p>EACH</p>

10-703		
SR 37 MOBILITY STUDY Town and Country Boulevard		
By: <u> BWS 4/24/12 </u>	Checked By: <u> BWC 11/24/12 </u>	
801-03290	CONSTRUCTION SIGN, C	2 EACH

<i>SUBTOTAL (THIS PAGE) =</i>		<i>2.0</i>
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TOTAL = 2.0

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: BWS 4/24/12

Checked By: BWC 11/24/12

801-04308

ROAD CLOSURE SIGN ASSEMBLY

4
EACH

SUBTOTAL (THIS PAGE) = 4.0

TOTAL = 4.0

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: BWS 4/24/12

Checked By: BWC 11/24/12

801-06625

DETOUR ROUTE MARKER ASSEMBLY

18
EACH

SUBTOTAL (THIS PAGE) = 18.0

TOTAL = 18.0

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: BWS 4/24/12

Checked By: BWC 11/24/12

801-06640

CONSTRUCTION SIGN, A

24
EACH

<i>SUBTOTAL (THIS PAGE) =</i>		<i>24.0</i>
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TOTAL = 24.0

Checked By: _____

4
EACH

TOTAL = 4.0

10-703

**SR 37 MOBILITY STUDY
Town and Country Boulevard**

By: BWS 4/24/12Checked By: BWC 11/24/12**801-06710****FLASHING ARROW SIGN****510
DAY**

<i>Description</i>						<i>Day</i>
<i>Assumptions: Used MOT Plan for 126th and Keystone as example MOT Plan.</i>						
<i>MOT Phase I</i>						
<i>Begin Project</i>						45
<i>Midde of project</i>						0
<i>End Project</i>						45
					<i>Total =</i>	90
<i>MOT Phase II</i>						
<i>Begin Project</i>						0
<i>Midde of project</i>						0
<i>End Project</i>						0
					<i>Total =</i>	0
<i>MOT Phase III</i>						
<i>Begin Project</i>						105
<i>Midde of project</i>						0
<i>End Project</i>						105
					<i>Total =</i>	210
<i>MOT Phase IV</i>						
<i>Begin Project</i>						105
<i>Midde of project</i>						0
<i>End Project</i>						105
					<i>Total =</i>	210
<i>MOT Phase V</i>						
<i>Begin Project</i>						0
<i>Midde of project</i>						0
<i>End Project</i>						0
					<i>Total =</i>	0
					<i>Total =</i>	510

SUBTOTAL (THIS PAGE) = 510.0

TOTAL = 510.0

10-703		
SR 37 MOBILITY STUDY Town and Country Boulevard		
By: <u> BWS 4/24/12 </u>	Checked By: <u> BWC 11/24/12 </u>	
801-06775	MAINTAINING TRAFFIC	1 LS

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/24/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
801-06775	MAINTAINING TRAFFIC	1 LS

10-703		
SR 37 MOBILITY STUDY Town and Country Boulevard		
By: <u> BWS 4/24/12 </u>	Checked By: <u> BWC 11/24/12 </u>	
801-06775	MAINTAINING TRAFFIC	1 LS

10-703		
SR 37 MOBILITY STUDY Town and Country Boulevard		
By: <u> BWS 4/24/12 </u>	Checked By: <u> BWC 11/24/12 </u>	
801-06775	MAINTAINING TRAFFIC	1 LS

10-703		
SR 37 MOBILITY STUDY Town and Country Boulevard		
By: <u> BWS 4/24/12 </u>	Checked By: <u> BWC 11/24/12 </u>	
801-06775	MAINTAINING TRAFFIC	1 LS

10-703		
SR 37 MOBILITY STUDY Town and Country Boulevard		
By: <u> BWS 4/24/12 </u>	Checked By: <u> BWC 11/24/12 </u>	
801-06775	MAINTAINING TRAFFIC	1 LS

10-703		
SR 37 MOBILITY STUDY Town and Country Boulevard		
By: <u> BWS 4/24/12 </u>	Checked By: <u> BWC 11/24/12 </u>	
801-06775	MAINTAINING TRAFFIC	1 LS

<i>SUBTOTAL (THIS PAGE) =</i>		<i>1.0</i>
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TOTAL = *1.0*

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: BWS 4/24/12

Checked By: BWC 11/24/12

801-07024

ENERGY ABSORBING TERMINAL, CZ, TL-3

1
EACH

<i>SUBTOTAL (THIS PAGE) =</i>		<i>1.0</i>
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TOTAL = 1.0

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: BWS 4/24/12

Checked By: BWC 11/24/12

801-07118

BARRICADE, III-A

228
LFT

SUBTOTAL (THIS PAGE) = 228.0

TOTAL = 228.0

<p>10-703</p> <p>SR 37 MOBILITY STUDY</p> <p>Town and Country Boulevard</p>		
<p>By: <u> BWS 4/24/12 </u></p>	<p>Checked By: <u> BWC 11/24/12 </u></p>	
<p>801-07119</p>	<p>BARRICADE, III-B</p>	<p>48</p> <p>LFT</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>BWS</i> <i>4/24/12</i> </u></p>	<p>Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u></p>	
<p>801-07119</p>	<p>BARRICADE, III-B</p>	<p>48</p> <p>LFT</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>BWS</i> <i>4/24/12</i> </u></p>	<p>Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u></p>	
<p>801-07119</p>	<p>BARRICADE, III-B</p>	<p>48</p> <p>LFT</p>

<p>10-703</p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS 4/24/12 </u></p>	<p>Checked By: <u> BWC 11/24/12 </u></p>	
<p>801-07119</p>	<p>BARRICADE, III-B</p>	<p>48</p> <p>LFT</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>BWS</i> <i>4/24/12</i> </u></p>	<p>Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u></p>	
<p>801-07119</p>	<p>BARRICADE, III-B</p>	<p>48</p> <p>LFT</p>

<p>10-703</p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS 4/24/12 </u></p>	<p>Checked By: <u> BWC 11/24/12 </u></p>	
<p>801-07119</p>	<p>BARRICADE, III-B</p>	<p>48</p> <p>LFT</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>BWS</i> <i>4/24/12</i> </u></p>	<p>Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u></p>	
<p>801-07119</p>	<p>BARRICADE, III-B</p>	<p>48</p> <p>LFT</p>

<i>SUBTOTAL (THIS PAGE) =</i>		<i>48.0</i>
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TOTAL = 48.0

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/23/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
801-08400	TEMPORARY TRAFFIC BARRIER, TYPE 1	3,009 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/23/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
801-08400	TEMPORARY TRAFFIC BARRIER, TYPE 1	3,009 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/23/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
801-08400	TEMPORARY TRAFFIC BARRIER, TYPE 1	3,009 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/23/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
801-08400	TEMPORARY TRAFFIC BARRIER, TYPE 1	3,009 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/23/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
801-08400	TEMPORARY TRAFFIC BARRIER, TYPE 1	3,009 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/23/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
801-08400	TEMPORARY TRAFFIC BARRIER, TYPE 1	3,009 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/23/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
801-08400	TEMPORARY TRAFFIC BARRIER, TYPE 1	3,009 LFT

<i>SUBTOTAL (THIS PAGE) =</i>		<i>3009.0</i>
<i>TOTAL =</i>		<i>3009.0</i>

TOTAL = 3009.0

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: BWS 4/23/12

Checked By: BWC 11/24/12

801-08507

TEMPORARY TRAFFIC BARRIER, TYPE 1. ANCHORED

296
LFT

<i>SUBTOTAL (THIS PAGE) =</i>		296.0
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TOTAL = 296.0

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: BWS 4/23/12

Checked By: BWC 11/24/12

801-08508

TEMPORARY TRAFFIC BARRIER, TYPE 2, ANCHORED

3,009
LFT

<i>SUBTOTAL (THIS PAGE) =</i>		<i>3009.0</i>
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TOTAL = 3009.0

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/24/12</i> </u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u>	
801-09133	TEMPORARY CHANGEABLE MESSAGE SIGN	2 EACH

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>BWS</i> <i>4/24/12</i> </u></p>	<p>Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u></p>	
<p>801-09133</p>	<p>TEMPORARY CHANGEABLE MESSAGE SIGN</p>	<p>2</p> <p>EACH</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>BWS</i> <i>4/24/12</i> </u></p>	<p>Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u></p>	
<p>801-09133</p>	<p>TEMPORARY CHANGEABLE MESSAGE SIGN</p>	<p>2</p> <p>EACH</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>BWS</i> <i>4/24/12</i> </u></p>	<p>Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u></p>	
<p>801-09133</p>	<p>TEMPORARY CHANGEABLE MESSAGE SIGN</p>	<p>2</p> <p>EACH</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>BWS</i> <i>4/24/12</i> </u></p>	<p>Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u></p>	
<p>801-09133</p>	<p>TEMPORARY CHANGEABLE MESSAGE SIGN</p>	<p>2</p> <p>EACH</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>BWS</i> <i>4/24/12</i> </u></p>	<p>Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u></p>	
<p>801-09133</p>	<p>TEMPORARY CHANGEABLE MESSAGE SIGN</p>	<p>2</p> <p>EACH</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>BWS</i> <i>4/24/12</i> </u></p>	<p>Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u></p>	
<p>801-09133</p>	<p>TEMPORARY CHANGEABLE MESSAGE SIGN</p>	<p>2</p> <p>EACH</p>

<i>SUBTOTAL (THIS PAGE) =</i>		<i>2.0</i>
--------------------------------------	--	-------------------

TOTAL = 2.0

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i>		
<i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/23/12</i></u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i></u>	
801-52817	TEMPORARY CROSSOVER, B	2 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/23/12</i> </u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u>	
801-52817	TEMPORARY CROSSOVER, B	2 EACH

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>BWS</i> <i>4/23/12</i> </u></p>	<p>Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u></p>	
<p>801-52817</p>	<p>TEMPORARY CROSSOVER, B</p>	<p>2</p> <p>EACH</p>

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>BWS</i> <i>4/23/12</i> </u></p>	<p>Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u></p>	
<p>801-52817</p>	<p>TEMPORARY CROSSOVER, B</p>	<p>2</p> <p>EACH</p>

<p>10-703</p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> BWS 4/23/12 </u></p>	<p>Checked By: <u> BWC 11/24/12 </u></p>	
<p>801-52817</p>	<p>TEMPORARY CROSSOVER, B</p>	<p>2</p> <p>EACH</p>

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>BWS</i> <i>4/23/12</i> </u>	Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u>	
801-52817	TEMPORARY CROSSOVER, B	2 EACH

<p><i>10-703</i></p> <p><i>SR 37 MOBILITY STUDY</i></p> <p><i>Town and Country Boulevard</i></p>		
<p>By: <u> <i>BWS</i> <i>4/23/12</i> </u></p>	<p>Checked By: <u> <i>BWC</i> <i>11/24/12</i> </u></p>	
<p>801-52817</p>	<p>TEMPORARY CROSSOVER, B</p>	<p>2</p> <p>EACH</p>

[illegible]

<i>SUBTOTAL (THIS PAGE) =</i>		<i>2.0</i>
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TOTAL = 2.0

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: JPS 11/28/12

Checked By: BWC 12/5/12

**802-05701 SIGN POST, SQUARE, TYPE 1, REINFORCED
ANCHOR BASE**

340
LFT

SUBTOTAL (THIS PAGE) = 340.0

TOTAL = 340.0

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: JPS 11/28/12

Checked By: BWC 12/5/12

802-07057

SIGN, PANEL, WITH LEGEND

429
SFT

SUBTOTAL (THIS PAGE) = 429.0

TOTAL = 429.0

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: JPS 11/28/12

Checked By: BWC 12/5/12

802-07138 WIDE FLANGE SIGN POST SUPPORT FOUNDATION,
IX

2
EACH

SUBTOTAL (THIS PAGE) = 2.0

TOTAL = 2.0

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: JPS 11/28/12

Checked By: BWC 12/5/12

802-07159

CANTILEVER SIGN SUPPORT FOUNDATION, II

2
EACH

SUBTOTAL (THIS PAGE) = 2.0

TOTAL = 2.0

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: JPS 11/28/12

Checked By: BWC 12/5/12

802-09840

SIGN, SHEET, WITH LEGEND 0.100 IN THICKNESS

115
SFT

SUBTOTAL (THIS PAGE) =		115.0
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TOTAL = 115.0

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: JPS 11/28/12

Checked By: BWC 12/5/12

802-76095

STRUCTURAL STEEL, BREAKAWAY

**681
LBS**

SUBTOTAL (THIS PAGE) = 680.2

TOTAL = 680.2

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: JPS 11/28/12

Checked By: BWC 12/5/12

802-76135

OVERHEAD SIGN STRUCTURE, CANTILEVER SINGLE ARM

**1
EACH**

SUBTOTAL (THIS PAGE) = 1.0

TOTAL = ***1.0***

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
804-06770	DELINEATOR POST	10 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
804-06770	DELINEATOR POST	10 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
804-06770	DELINEATOR POST	10 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
804-06770	DELINEATOR POST	10 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
804-06770	DELINEATOR POST	10 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
804-06770	DELINEATOR POST	10 EACH

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i>		
By: <u> <i>srs</i> <i>11/19/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
804-06770	DELINEATOR POST	10 EACH

<i>SUBTOTAL (THIS PAGE) =</i>		<i>10.0</i>
<i>TOTAL =</i>		<i>10.0</i>

<i>SUBTOTAL (THIS PAGE) =</i>		<i>10.0</i>
<i>TOTAL =</i>		<i>10.0</i>

<p style="text-align: center;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> DJZ 4/17/12 </u> Checked By: <u> BWS 11/24/12 </u></p>		
805-02087	TRANSPORTATION OF SALVAGEABLE SIGNAL EQUIPMENT	1 LS

<p style="text-align: center;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> DJZ 4/17/12 </u> Checked By: <u> BWS 11/24/12 </u></p>		
805-02087	TRANSPORTATION OF SALVAGEABLE SIGNAL EQUIPMENT	1 LS

<p style="text-align: center;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> DJZ 4/17/12 </u> Checked By: <u> BWS 11/24/12 </u></p>		
805-02087	TRANSPORTATION OF SALVAGEABLE SIGNAL EQUIPMENT	1 LS

<p style="text-align: center;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> DJZ 4/17/12 </u> Checked By: <u> BWS 11/24/12 </u></p>		
805-02087	TRANSPORTATION OF SALVAGEABLE SIGNAL EQUIPMENT	1 LS

<p style="text-align: center;"><i>10-703</i></p> <p style="text-align: center;"><i>SR 37 MOBILITY STUDY</i> <i>Town and Country Boulevard</i></p> <p>By: <u> DJZ 4/17/12 </u> Checked By: <u> BWS 11/24/12 </u></p>		
805-02087	TRANSPORTATION OF SALVAGEABLE SIGNAL EQUIPMENT	1 LS

[illegible]

<i>SUBTOTAL (THIS PAGE) =</i>		<i>1.0</i>
<i>TOTAL =</i>		<i>1.0</i>

TOTAL = 1.0

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: DJZ 5/8/12

Checked By: BWS 11/24/12

808-10031

LINE, MULTI-COMPONENT, BROKEN, WHITE, 4 IN

**2,528
LFT**

TOTAL =	2527.8
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10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: DJZ 5/8/12Checked By: BWS 11/24/12**808-10033****LINE, MULTI-COMPONENT, SOLID, WHITE, 4 IN****9,695
LFT**

<i>Begin Station</i>	<i>End Station</i>	<i>Begin Offset</i>	<i>End Offset</i>			<i>Length (ft)</i>
Line "A"	NB Outside					
238+33.71	239+39.58	50.62	50.62			106
239+39.58	240+39.58	50.62	62.00			101
240+39.58	245+01.18	62.00	45.21			462
245+01.18	250+38.46	45.21	64.75			538
250+38.72	263+73.31	31.25	31.25			1335
263+73.74	266+70.35	64.75	43.25			297
266+70.35	268+42.54	43.25	43.25			172
	SB Outside					
238+33.71	241+46.03	-62.35	-69.87			312
241+46.03	245+48.05	-69.87	-55.25			402
245+48.05	247+48.84	-55.25	-55.25			201
247+48.84	250+45.58	-55.25	-76.75			298
250+46.46	263+81.00	-31.25	-31.25			1335
263+81.00	268+42.63	-76.75	-76.75			462
Line "S-2-A"		<i>Right</i>				
10+36.66	11+51.19	16.00	20.85			115
12+20.91	17+05.14	59.96	21.60			486
		<i>Left</i>				
10+17.22	14+94.12	-16.00	-59.97			479
15+45.62	17+21.61	-32.39	-29.19			176
RAMPS	SE					
10+00.00	10+60.24	0.00	12.00			61
10+60.24	11+59.99	12.00	24.00			100
11+59.99	14+35.65	24.00	24.00			276
14+35.65	14+89.13	24.00	27.71			54
14+45.86	16+11.01	12.00	12.00			165
	NE					
11+54.55	12+14.13	13.50	12.00			60
12+14.13	16+09.51	12.00	12.00			395
	SW					
10+00.00	12+14.13	12.00	12.00			214
11+62.67	16+07.65	26.80	24.00			445
	NW					
10+00.00	14+33.64	24.00	24.00			434
14+33.64	14+84.52	24.00	27.52			51
14+43.86	16+09.00	12.00	12.00			165

TOTAL = 9694.9

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: DJZ 5/8/12 Checked By: BWS 11/24/12 **808-10034****LINE, MULTI-COMPONENT, SOLID, YELLOW, 4 IN****9,328
LFT**

<i>Begin Station</i>	<i>End Station</i>	<i>Begin Offset</i>	<i>End Offset</i>			<i>Length (ft)</i>
Line "A"	NB Inside					
238+33.72	240+21.94	26.62	26.62			188
240+21.94	245+54.57	26.62	7.25			533
245+54.57	255+61.08	7.25	7.25			1007
Pavement Exception						
258+58.68	268+42.63	7.25	7.25			984
Line "A"	SB Inside					
238+33.72	240+21.94	-26.35	-26.35			188
240+21.94	245+47.17	-26.35	-7.25			526
245+47.17	255+61.63	-7.25	-7.25			1014
Pavement Exception						
258+58.95	268+42.63	-7.25	-7.25			984
SE Ramp						
10+00.00	16+10.98	0.00	0.00			611
NE Ramp						
10+00.00	16+09.51	0.00	0.00			610
NW Ramp						
10+00.00	16+09.01	0.00	0.00			609
SW Ramp						
10+00.00	16+07.65	0.00	0.00			608
Line "S-2-A"	EB					
10+17.22	10+36.66	4.00	4.00			19
10+36.66	11+00.48	4.00	-1.02			64
11+00.48	11+24.64	-1.02	-1.89			24
11+24.64	11+48.29	-1.89	3.06			24
11+78.23	11+87.53	0.00	27.40			29
11+87.53	12+22.95	27.40	45.00			40
12+22.95	12+53.35	45.00	33.43			33
12+53.35	12+86.20	33.43	13.72			38
12+86.20	13+24.05	13.72	7.80			38
13+24.05	13+62.18	7.80	7.80			38
13+62.18	13+91.07	0.00	0.85			29
13+91.07	14+28.42	0.85	9.36			38
14+28.42	14+59.83	9.36	31.28			38

SUBTOTAL (THIS PAGE) = 8313.8

SR 37 MOBILITY STUDY
Town and Country Boulevard

Checked By: _____

LFT

TOTAL = 9327.6

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>5/8/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
808-10037	LINE, MULTI-COMPONENT, SOLID, WHITE, 8 IN	3,816 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>5/8/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
808-10037	LINE, MULTI-COMPONENT, SOLID, WHITE, 8 IN	3,816 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>5/8/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
808-10037	LINE, MULTI-COMPONENT, SOLID, WHITE, 8 IN	3,816 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>5/8/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
808-10037	LINE, MULTI-COMPONENT, SOLID, WHITE, 8 IN	3,816 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>5/8/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
808-10037	LINE, MULTI-COMPONENT, SOLID, WHITE, 8 IN	3,816 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>5/8/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
808-10037	LINE, MULTI-COMPONENT, SOLID, WHITE, 8 IN	3,816 LFT

<i>10-703</i>		
<i>SR 37 MOBILITY STUDY Town and Country Boulevard</i>		
By: <u> <i>DJZ</i> <i>5/8/12</i> </u>	Checked By: <u> <i>BWS</i> <i>11/24/12</i> </u>	
808-10037	LINE, MULTI-COMPONENT, SOLID, WHITE, 8 IN	3,816 LFT

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: DJZ 5/8/12

Checked By: BWS 11/24/12

808-75071

PAVEMENT MESSAGE MARKING, PREFORMED PLASTIC, LANE INDICATION ARROW

8
EACH

<i>SUBTOTAL (THIS PAGE) =</i>		8.0
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TOTAL = 8.0

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: DJZ 5/8/12

Checked By: BWS 11/24/12

**808-75510 TRANSVERSE MARKINGS, PREFORMED PLASTIC,
CROSSHATCH LINE, WHITE, 24 IN**

538
LFT

[illegible]

<i>SUBTOTAL (THIS PAGE) =</i>		<i>537.2</i>
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TOTAL = 537.2

10-703

SR 37 MOBILITY STUDY
Town and Country Boulevard

By: DJZ 5/8/12 Checked By: BWS 11/24/12 **808-75998****SNOWPLOWABLE RAISED PAVEMENT MARKER****352
EACH**

<i>Begin Station</i>	<i>End Station</i>	<i>Begin Offset</i>	<i>End Offset</i>	<i>Length</i>	<i>Spacing</i>	<i>Each</i>
Line "A"	NB	<i>(One-way white markers at broken white lane lines)</i>				
238+33.72	240+22.16	38.62	38.62	188.44	80	3
240+22.16	245+54.79	38.62	19.25	532.98	80	7
245+54.79	255+60.63	19.25	19.25	1005.84	80	13
Pavement Exception						
258+57.95	268+42.63	19.25	19.25	984.68	80	13
240+39.58	245+55.01	50.00	31.25	515.77	80	7
245+55.01	245+69.21	31.25	31.25	14.20	80	1
Line "A"	SB					
238+33.71	240+22.16	-38.35	-38.35	188.45	80	3
240+22.16	245+47.39	-38.35	-19.25	525.58	80	7
245+47.39	255+62.08	-19.25	-19.25	1014.69	80	13
Pavement Exception						
258+59.40	268+42.63	-19.25	-19.25	983.23	80	13
238+33.71	240+22.38	-50.35	-50.35	188.67	80	3
240+22.38	245+47.61	-50.35	-31.25	525.58	80	7
245+47.61	247+77.32	-31.25	-31.25	229.71	80	3
241+46.03	245+47.83	-57.86	-43.25	402.07	80	6
245+47.83	247+49.27	-43.25	-43.25	201.44	80	3
247+49.27	250+46.02	-43.25	-64.75	297.53	80	4
SE Ramp						
11+60.00	14+45.86	12.00	12.00	285.86	40	8
14+45.86	15+34.01	12.00	15.08	88.20	40	3
15+34.01	16+11.01	15.08	16.00	77.01	40	2
NW Ramp						
10+00.00	14+43.86	12.00	12.00	443.86	40	12
14+43.86	15+32.01	12.00	15.08	88.20	40	3
15+32.01	16+05.00	15.08	15.80	72.99	40	2
SW Ramp						
10+00.00	11+01.00	15.30	14.50	101.00	40	3
11+01.00	12+14.13	14.50	12.00	113.16	40	3
12+14.13	16+07.65	12.00	12.00	393.52	40	10

SUBTOTAL (THIS PAGE) = 152.0

10-703

**SR 37 MOBILITY STUDY
Town and Country Boulevard**

By: BWS 4/20/12

Checked By: _____

808-75998

SNOWPLOWABLE RAISED PAVEMENT MARKER**EACH**

<i>Begin Station</i>	<i>End Station</i>	<i>Begin Offset</i>	<i>End Offset</i>	<i>Length</i>	<i>Spacing</i>	<i>Each</i>
Line "S-2-A"	EB	(One-way white markers at solid white lane lines thru roundabout)				
10+17.22	10+36.66	16.00	16.00	19.44		
10+36.66	10+94.33	16.00	12.00	57.81		
10+94.33	11+23.41	12.00	12.26	29.08		
11+23.41	11+51.19	12.26	20.85	29.08		
11+69.50	11+63.88	-26.70	-8.79	18.77		
11+63.88	11+63.23	-8.79	0.00	8.81		
11+63.23	11+69.66	0.00	27.03	27.78		
11+69.66	11+80.04	27.03	41.65	17.93		
12+20.91	12+43.33	60.00	56.53	22.69		
12+43.33	12+62.89	56.53	45.00	22.71		
12+62.89	12+95.43	45.00	25.60	37.88		
12+95.43	13+32.75	25.60	20.14	37.72		
13+32.75	13+81.54	20.14	12.56	49.38		
13+81.54	14+18.38	12.56	20.80	37.75		
14+18.38	14+49.52	20.80	42.20	37.78		
14+49.52	14+66.16	42.20	54.00	20.40		
14+66.16	14+85.82	54.00	59.70	20.47		
14+85.82	15+02.20	59.70	59.20	16.39		
15+02.20	15+17.80	59.20	54.30	16.35		
15+17.80	15+71.44	54.30	33.50	57.53		
15+71.44	16+24.13	33.50	21.20	54.11		
16+24.13	16+48.56	21.20	17.23	24.75		
16+48.56	16+77.11	17.23	16.00	28.58		
16+77.11	17+07.52	16.00	22.38	31.07		
	WB		Total Length =	724.26	40	19
10+17.22	10+26.90	-16.00	-16.00	9.68		
10+26.90	11+12.81	-16.00	-25.33	86.42		
11+12.81	11+97.62	-25.33	-54.26	89.61		
11+97.62	12+29.98	-54.26	-59.62	32.80		
12+29.98	12+49.60	-59.62	-53.90	20.44		
12+49.60	12+66.16	-53.90	-41.91	20.44		
12+66.16	12+97.17	-41.91	-20.36	37.76		
12+97.17	13+33.95	-20.36	-11.84	37.75		
13+33.95	13+53.23	-11.84	-11.11	19.29		
13+53.23	13+82.53	-11.11	-19.43	30.46		
13+82.53	14+19.82	-19.43	-25.40	37.76		
14+19.82	14+52.23	-25.40	-44.77	37.76		
14+52.23	14+71.71	-44.77	-56.40	22.69		
14+71.71	14+94.12	-56.40	-59.97	22.69		
			Total Length =	505.56	40	13

SUBTOTAL (THIS PAGE) =

32.0

SR 37 MOBILITY STUDY
Town and Country Boulevard

Checked By: _____

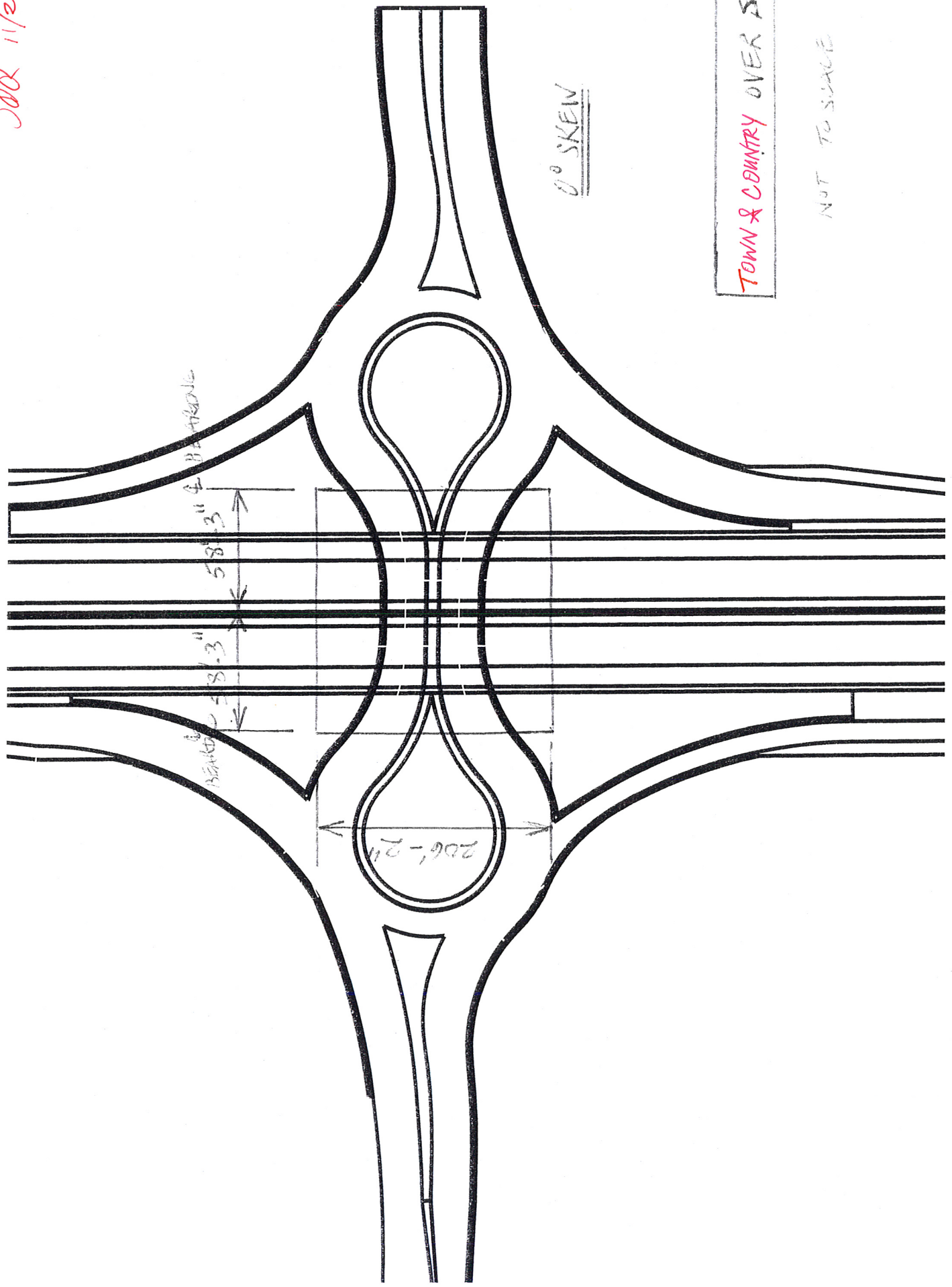
EACH

<i>SUBTOTAL (THIS PAGE) =</i>		<i>104.0</i>
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BRIDGE QUANTITIES

BRIDGE GEOMETRY
TOWN & COUNTRY RD. OVER SR 37

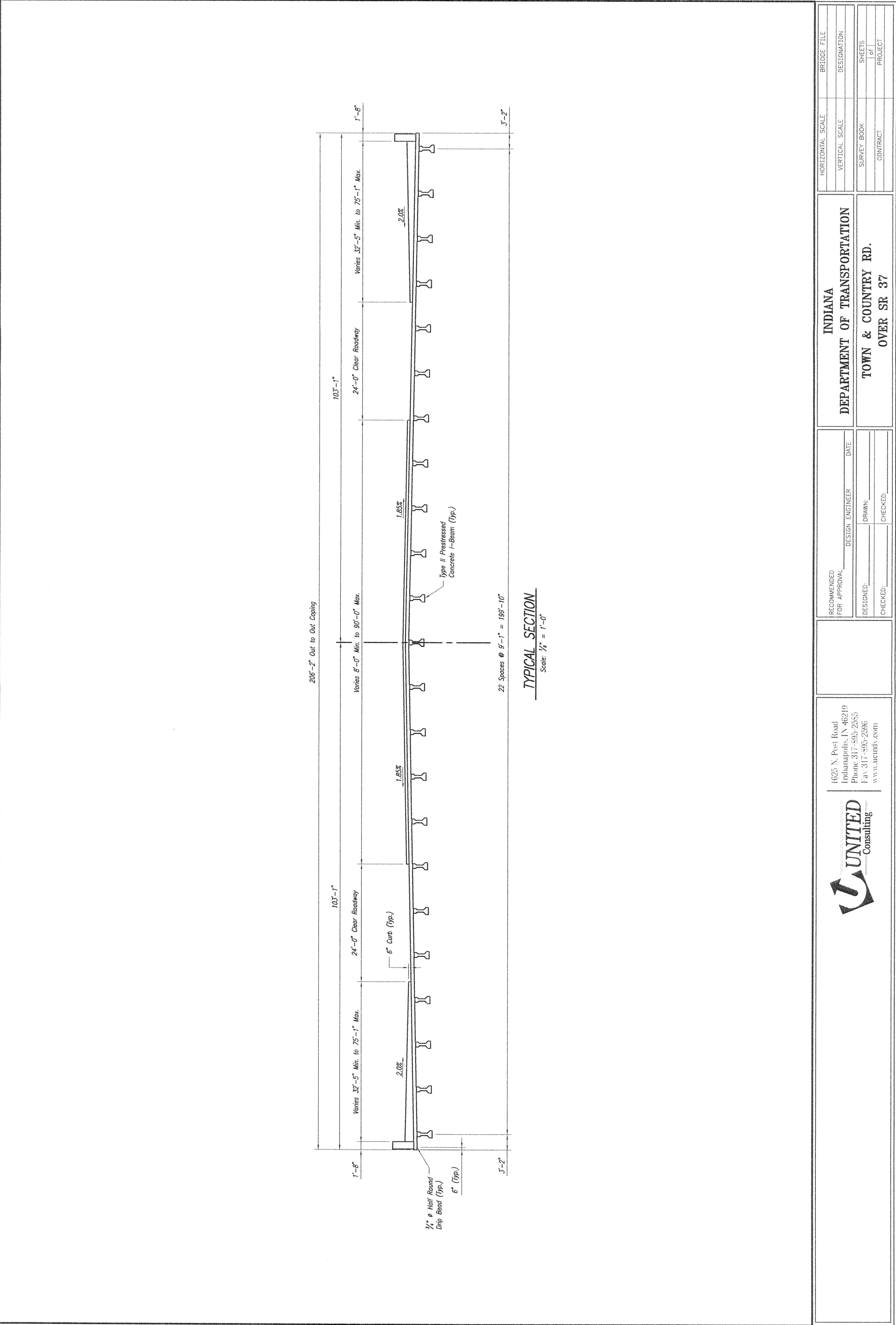
11/15/12
JRR 11/21/12



0° SKEW

TOWN & COUNTRY OVER SR 37

NOT TO SCALE



File Name: P:\C30\10-703\Bridge\Design\Primary Design\Town & Country Over SR37 All Typical Section.dwg Plot Date: 11/19/2012 Plotted By: Mallory Andrew L.



1625 N. Post Road
Indianapolis IN 46219
Phone 317-595-2550
Fax 317-595-2596
www.uicindy.com

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
	DRAWN:	CHECKED:

INDIANA
DEPARTMENT OF TRANSPORTATION
TOWN & COUNTRY RD.
OVER SR 37

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION
SURVEY BOOK	SHEETS
CONTRACT	of
	PROJECT

Town&Country Rd. Over SR37

Des by JTB 11/12/2012

Chk by

Rev by

11/14/12

Summary of Bridge Quantities

Structure Number

INDOT Item Code	Item Description	unit	Quantity	
105-06845	CONSTRUCTION ENGINEERING	LS	3%	
110-01001	MOBILIZATION AND DEMOBILIZATION	LS	5%	
203-02020	EXCAVATION, FOUNDATION, UNCLASSIFIED	CYS	557	
211-02050	B BORROW	CYS	557	
302-07455	DENSE GRADED SUBBASE	CYS	156	
609-06259	REINFORCED CONCRETE BRIDGE APPROACH, 12 IN.	SYS	939	
701-06011	DYNAMIC PILE LOAD TEST	EACH	3	
701-09559	TEST PILE, DYNAMIC, RESTRIKE	EACH	3	
701-09690	TEST PILE, DYNAMIC, 14 IN NON-PRODUCTION	LFT	210	
701-08122	PILE, STEEL PIPE, 0.375", 14	LFT	5,580	
702-51005	CONCRETE,A,SUBSTRUCTURE	CYS	300	
702-51015	CONCRETE,B,FOOTINGS	CYS	206	
703-06028	REINFORCING BARS	LBS	61,660	
703-06029	REINFORCING BARS, EPOXY COATED	LBS	443,858	
704-51002	CONCRETE, C, SUPERSTRUCTURE	CYS	1,696	
706-09959	RAILING, CONCRETE, FT	LFT	236	
707-05983	STRUCTURAL MEMBERS, CONCRETE I-BEAM, II, 36 IN. X 12 IN.	LFT	2,680	
709-51821	SURFACE SEAL	SFT	29,198	estimated

Town&Country Rd. over SR 37

Des by JTB 11/12/2012

Chk by JTB 11/14/12

Rev by

Proposed Structure # is _____

Town & Country Rd. over SR 37

Design Standards = Road Over Under
4R 4R

Functional Classification = Urban Collector Urban Arterial

ADT = xxxx (yr. 2030) xxxx

Design Speed = 35 mph 55

Vertical Clearance Req'd = 16.5 feet

Skew = 0 degrees

Calculated C-C End Brg. Length = 116.5 feet

USE 116.5 feet

Span Configuration Anticipated = 1 @ 58.25 feet
1 @ 58.25 feet

Town&Country Rd. over SR 37

Des by JTB 11/12/2012
Chk by JTB 11/14/12
Rev by

Proposed Structure # is 0

Town & Country Rd. over SR 37

Number of Spans = 2 spans

O-O Coping Width = 206.17 feet

C-C End Brg Length = 116.5 feet

Skew = 0.0000 degrees

O-O Bridge Length = 118.0 feet

Clear Roadway Width = 202.83 feet

Slab Thickness = 8 inches

Number of Piers units = 1

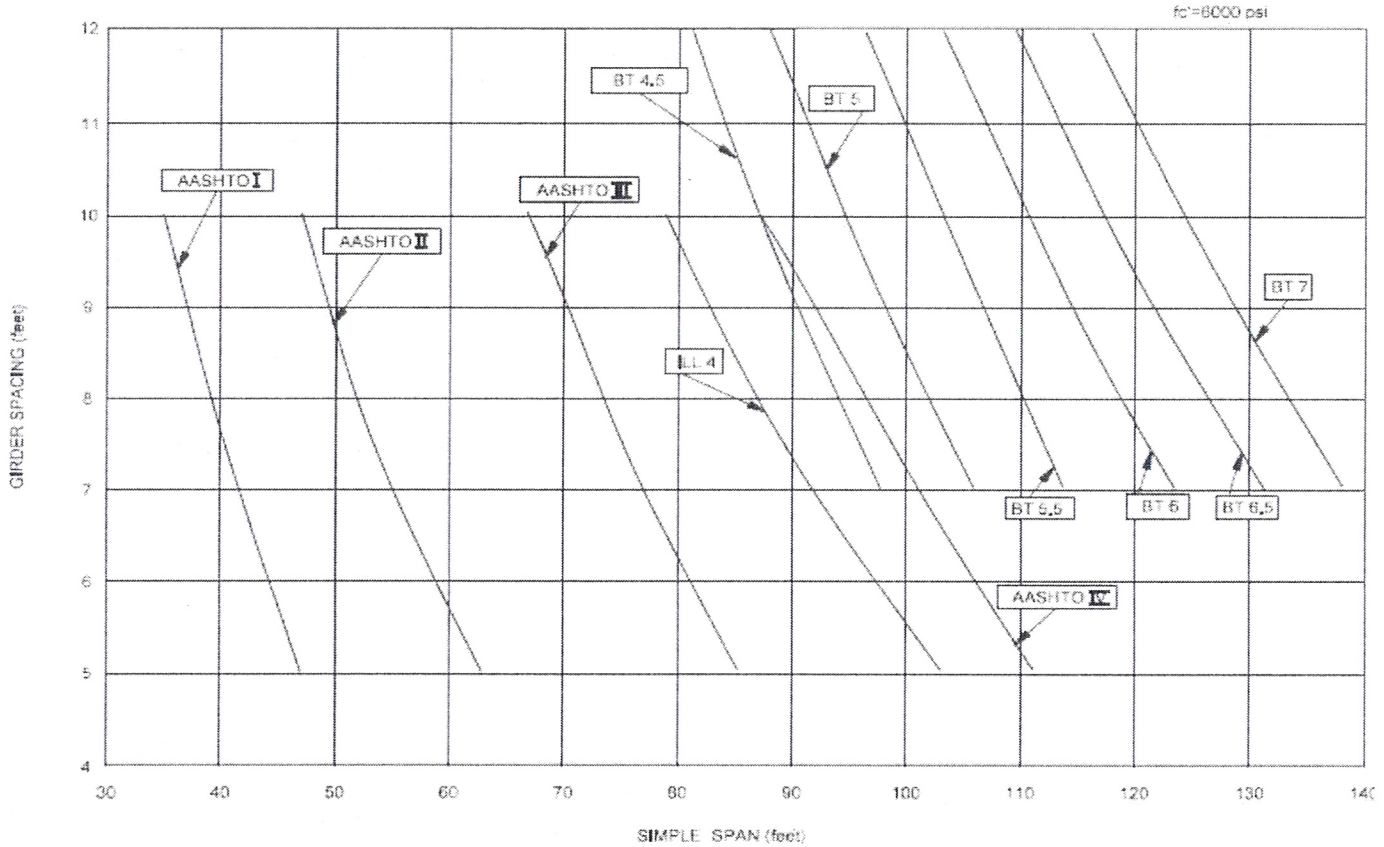
Number of Substructure units = 3

Twin Structure = NO

Type of Slope Wall = MSE Wall

Beam Quantities

Structure Number 0
Town & Country Rd. over SR 37



PRESTRESSED CONCRETE I-BEAM SELECTION CHART

Figure 59-3K

Beam Type = STRUCTURAL MEMBERS, CONCRETE I-BEAM, II, 36 IN. X 12 IN.

Overhang to be = 3.1705 ft
Spacing to be = 9.083 ft

out to out width = 206.17 ft
Beam Length = 116.50 ft
Beams Needed = 23 ft

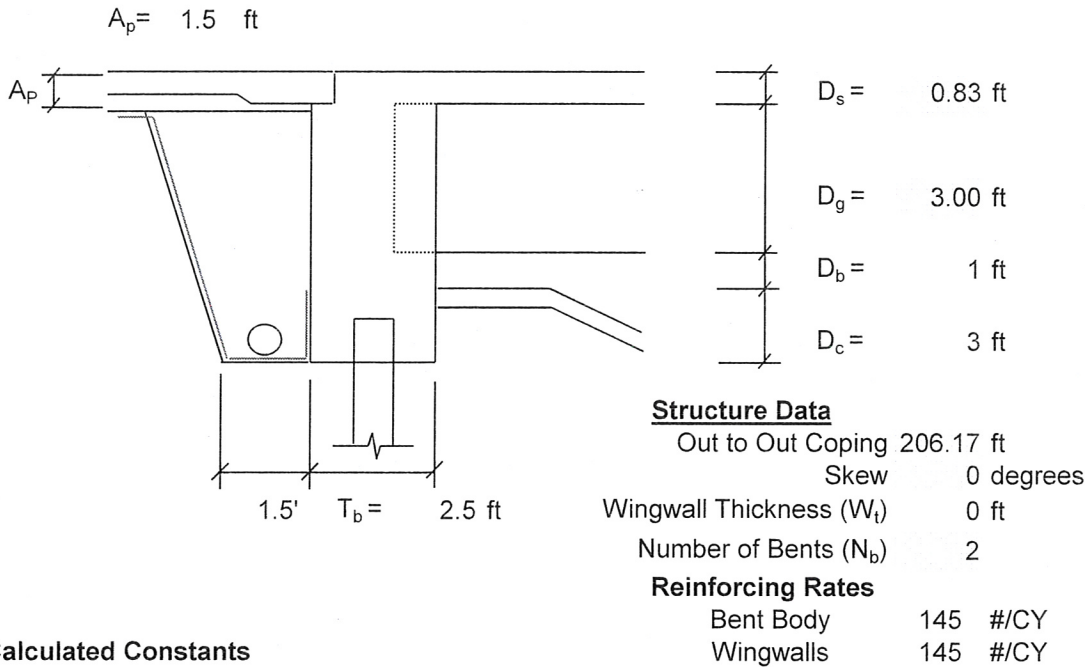
Twin Structure = NO

Length Needed = 2,680 ft

Bent Quantities

Structure Number 0

Town & Country Rd. over SR 37



Calculated Constants

Bent Length (L_B) = O-to-O Coping / $\cos(\text{skew}) = 206.17$ ft

Total Bent Depth (D_T) = $D_s + D_g + D_b + D_c = 7.83$ ft

Wing Length (W_L) = $(D_T - D_c) * 2 + 1' = 0.00$ ft

Concrete Quantities

Class C, Superstructure

Bent Body $V_B = N_b * (T_b * (D_T - D_s) * L_B) / 27$

$V_B = 267.3$ cubic yards

Wingwalls $V_W = N_b * (2 * D_T * W_L * W_t) / 27$

$V_W = 0.0$ cubic yards

Total Class C, Superstructure 267.3 cubic yards

Epoxy Coated Reinforcing Bar Quantities

Bent Body	38,752	lbs.
Wingwalls	00	lbs.
Total	38,752	lbs.

Piling Quantities

Number of Piles per Bent	23	piles
Estimated Pile Length	60	ft.
Total Length of Piles	2,760	linear feet

Pipe, End Bent Drain, 6" = $N_b * (L_B + 2 * (W_L + 3 * D_T)) = 0.0$ ft.

Geotextiles = $N_b * ((D_T - A_p) * 1.031 + 4.5') * L_B / 9 = 0.0$ sys

Aggregate for End Bent Backfill

$V_{bf} = ((D_T - A_p) / 4 + 1.5) * 1.5 / 2 * (D_T - A_p) * L_B * N_b$

$V_{bf} = 0$ cubic yards

Town & Country Rd. over SR 37

Des by JTB 11/12/2012

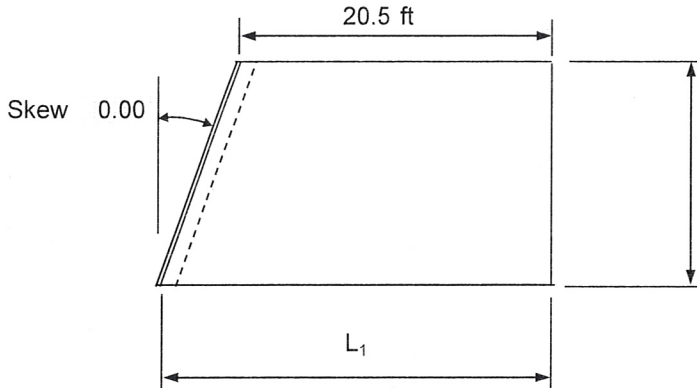
Chk by 10/21/12

Rev by

Approach Slab Quantities

Structure Number 0
Town & Country Rd. over SR 37

Number or Approach Slabs 2



$$L_1 = 20.5 + (W_c \times \tan \text{skew})$$

$$L_1 = 20.50 \text{ ft.}$$

$$D_{\text{sub}} = \text{Depth of Aggregate (inches)} = 6$$

$$\text{Reinforced Concrete Bridge Approach (A)} = (L_1 + 20.5)/2 * W_c / 9$$

$$A = 470 \text{ sys per approach}$$

$$\text{Dense Graded Subbase (T}_{\text{base}}) = A * D_{\text{sub}}$$

$$T_{\text{base}} = 78.0 \text{ cys per approach}$$

Epoxy Coated Reinforcing Bars

Reinforcement Rates 35 #/sy

Total Weight 16,436 Lbs.

per approach

Grand Totals

A = 939 sys

T_{base} = 156 cys

Reinforcing 32,872 Lbs.

Town & Country Rd. over SR 37

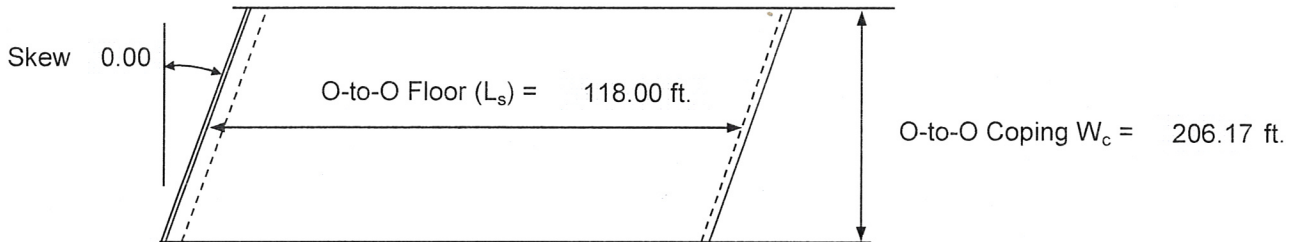
Des by JTB 11/12/2012

Chk by JDR 11/14/12

Rev by _____

Deck Quantities

Structure Number 0
Town & Country Rd. over SR 37



Slab Thickness T_s = 8.00 inches

Coping Depth D_c = 9 inches

Clear Roadway Width CR = 202.8 ft

Concrete Quantities

Class C, Superstructure

$$\text{Deck Slab } V_D = (L_s * W_c * T_s) / 27$$

$$V_D = 600.7 \text{ cubic yards}$$

$$\text{Sidewalk } V_s = (L_s * (W_c - 60') * 1') / 27$$

$$V_s = 638.8 \text{ cubic yards}$$

Concrete in fillets over beams and in thickened copings
Increase deck concrete by 15%

$$V_T = 1428.7 \text{ cys cubic yards}$$

Twin Structure = NO

of Bridge Rail Trans = 4

Pier Diaphragm - Class, C, Superstr.
(add to Concrete, C, Superstructure)

$$\text{Vol} = 3.83' * 3.5' * 199.66' / 27 = 99.1 \text{ cys}$$

Bridge Railing

$$\text{Area of Rail } A_r = 3.64 \text{ Sq. Ft.}$$

$$\text{Perimeter } P = 8.65 \text{ Ft.}$$

$$\text{LFT} = 236$$

$$V_R = (L_s * A_r) / 27$$

$$V_R = 31.9 \text{ cubic Yards}$$

Surface Seal

$$\text{Deck} = L_s * W_c = 24328 \text{ square feet}$$

$$\text{Coping} = L_s * D_c * 2 = 787 \text{ square feet}$$

$$\text{Rail} = L_s * P * 2 = 4083 \text{ square feet}$$

$$\text{Total} = 29,198 \text{ square feet}$$

Epoxy Coated Reinforcing Bars

Reinforcement Rates 250 #/cy

Deck 250 #/cy

Rail 330 #/cy

Deck 357175 Lbs.

Rail 10527 Lbs.

Trans. 4532 Lbs.

Total Weight 372,234 Lbs.

Grates, Basins, and Fittings, Cast Iron

$$N_G = 0 \text{ each}$$

Weight per Drain = 1000 Lbs.

Total Weight 0 Lbs.

Roadway Drain (SQ or OS)

$$N_G = 0 \text{ each}$$

Town & Country Rd. over SR 37

Des by JTB 11/12/2012

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Rev by _____

Excavation Quantities

Structure Number 0
Town & Country Rd. over SR 37

Number of Piers $N_p = 1$

$D_c = 6$ ft.

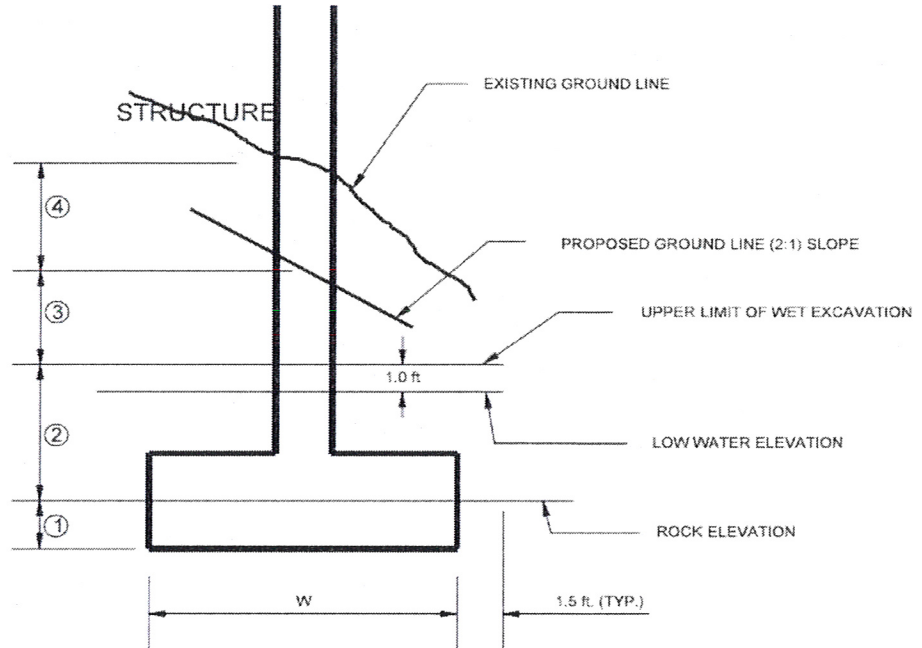
$D_{dry} = 0$ ft.

$D_{wet} = 0$ ft.

$D_x = 0$ ft.

$W = 9$ ft.

$L = 206$ ft.



Class X Excavation (V_x) = $N_p \times L \times W \times D_x / 27 = 0$ cubic yards

Wet Excavation (V_{wet}) = $N_p \times (L+3) \times (W+3) \times (D_{wet}) / 27 = 0$ cubic yards

Dry Excavation (V_{dry}) = $N_p \times (L+3) \times (W+3) \times (D_{dry}) / 27 = 0$ cubic yards

Fnd. Exc.(Unclass.) (V_c) = $N_p \times (L+3) \times (W+3) \times (D_c) / 27 = 557$ cubic yards

Is this structure over a waterway? **No**

B-Borrow (V_b) = Sum of Excavation Items = 557 cubic yards

Rev. 9/2/09

Town & Country Rd. over SR 37

Des by JTB 11/12/2012

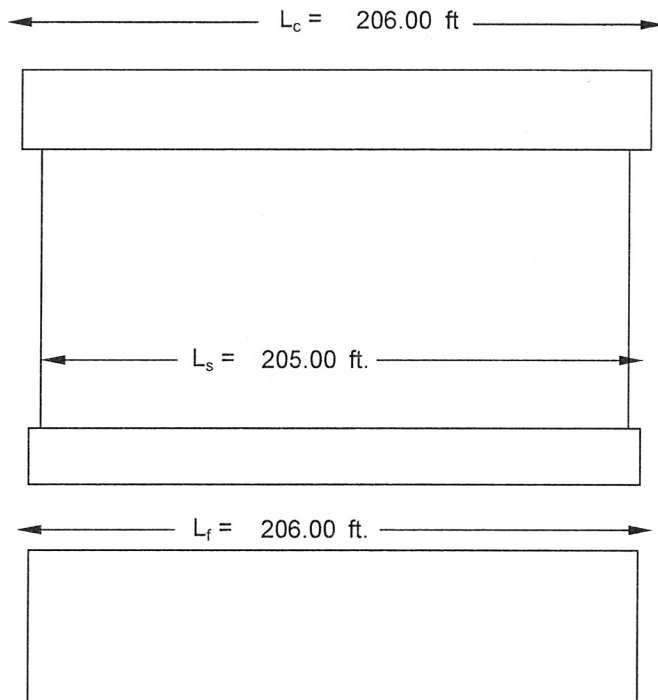
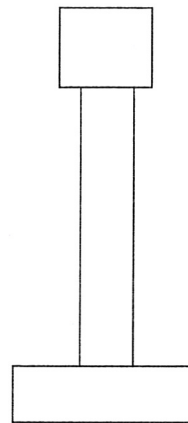
Chk by JTB 11/14/12

Rev by _____

Pier Quantities

Structure Number 0

Town & Country Rd. over SR 37

 $W_c = 3$ ft

Number of Piers 1

Reinforcing Rates

Footings 110 #/CY

Stem & Cap 130 #/CY

Concrete Quantities

Class B, Footing	$V_B = L_f \times W_f \times H_f \times 1/27$
	$V_B = 206.0$ cubic yards
Class A, Substructure	$V_A = (L_s \times W_s \times H_s + L_c \times W_c \times H_c) \times 1/27$
	$V_A = 300.0$ cubic yards

Reinforcing Bar Quantities

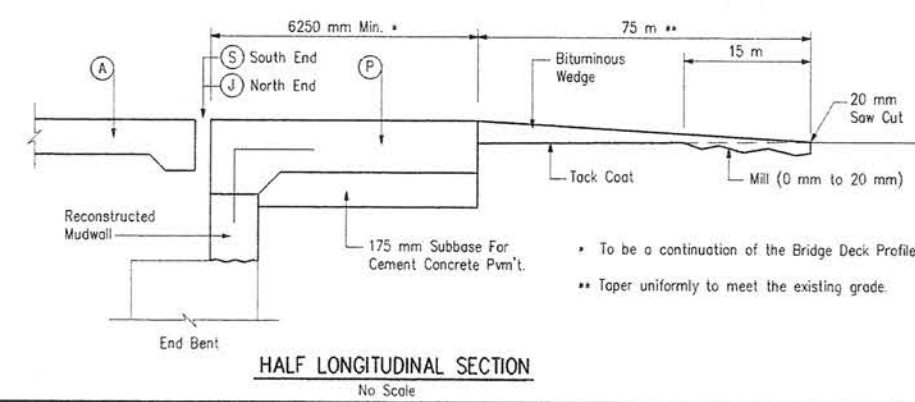
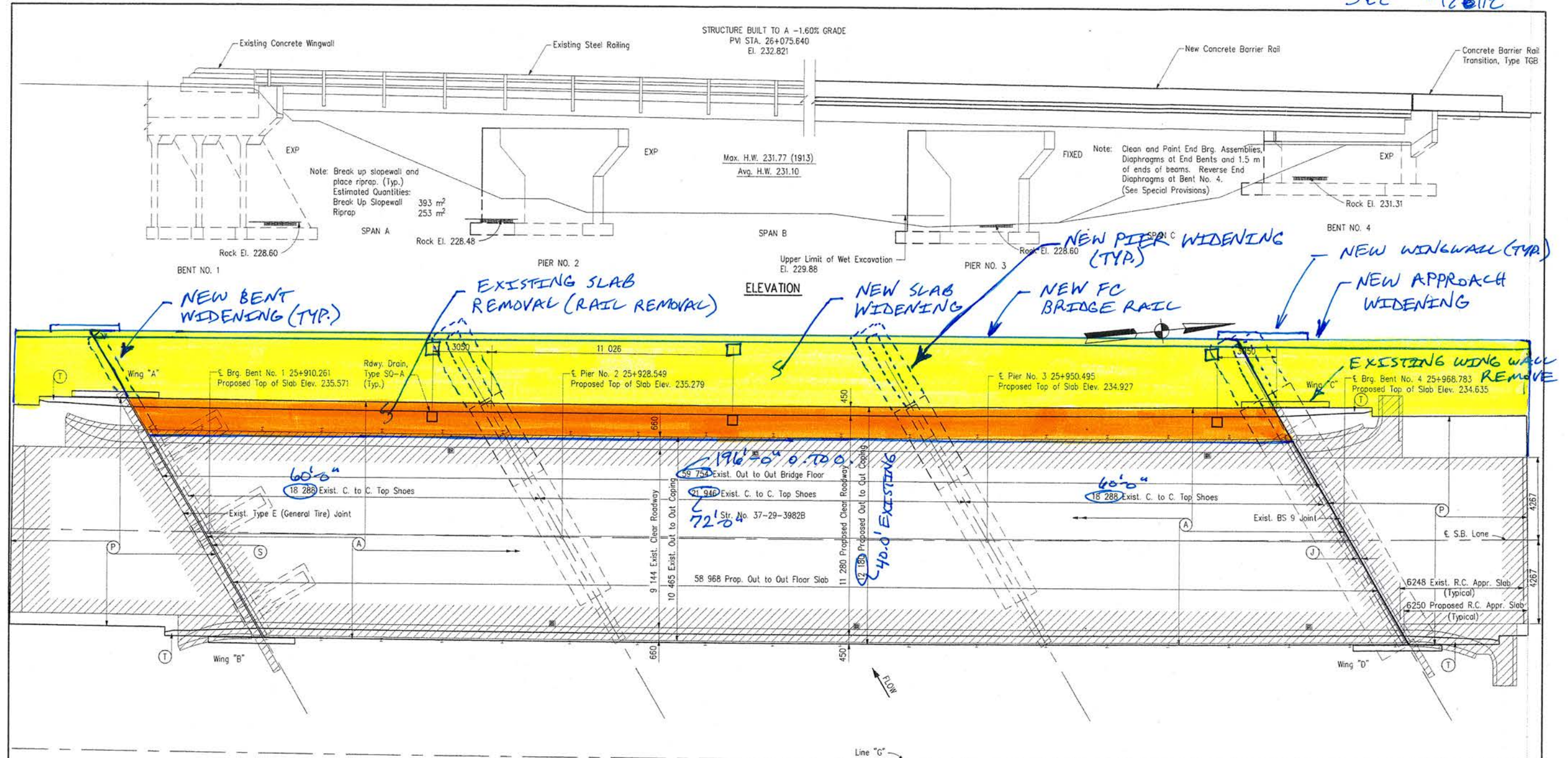
Footings	22,660	lbs.
Stem and Cap	39,000	lbs.
Total	61,660	lbs.

Piling Quantities

Est of Piles per Pier	50	piles
Estimated Pile Length	60	ft.
Total Length of Piles	3,000	linear feet

BRIDGE GEOMETRY
SR 37 OVER STONEY CREEK

JTB 11/26/12
JEC 11/26/12



BRIDGE WIDENING PLAN SOUTHBOUND STRUCTURE SHOWN ABOVE NORTHBOUND STRUCTURE (SIMILAR)

- NOTES**
- For Structure Section, See Dwg. D3.
 - For Approach Work (Guardrail, Wedge, etc.) See Sheet 5.
 - Stationing and Elevations from Original Plans. Contractor to Verify in field.

LEGEND

(A) 200 mm R.C. Deck Slab	(S) Expansion Joint, Class SS
(J) Expansion Joint Sealing System	(T) Conc. Barrier Rail Transition, Type TGB
(P) 250 mm R.C. Pavement	(X) Concrete Removal Items

RECONSTRUCTION AND WIDENING TO
CONTINUOUS COMPOSITE STEEL BEAM BRIDGE
3 SPANS @ 18 288, 21 946 & 18 288 30' SKEW LEFT
11 280 CLEAR ROADWAY CONCRETE BARRIER RAILS
SOUTHBOUND S.R. 37 OVER STONEY CREEK

DWG. D1 OF 21



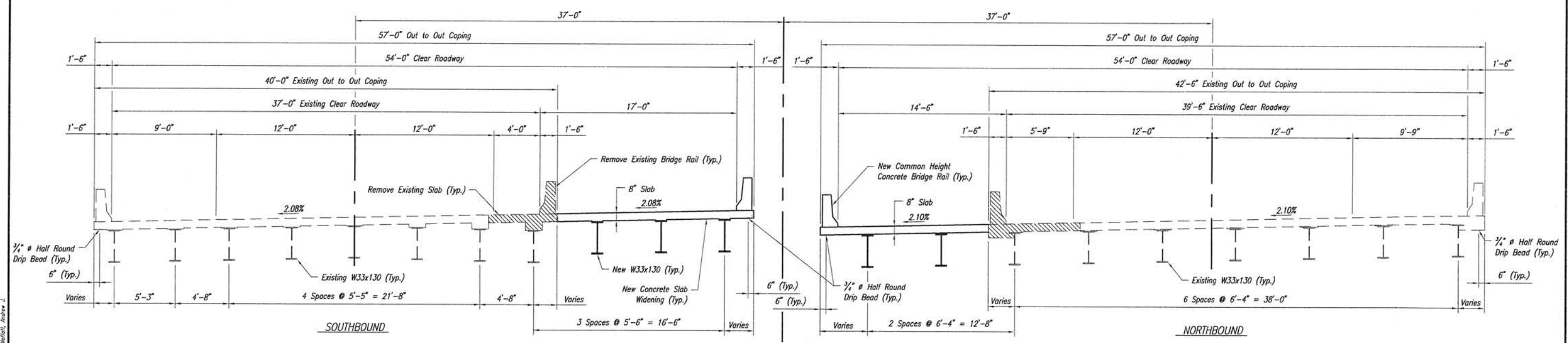
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: DMS	DRAWN: CPM	
CHECKED: DRC	CHECKED: DRC	

INDIANA
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN
SOUTHBOUND STRUCTURE

HORIZONTAL SCALE	BRIDGE FILE
1:100	37-29-3982 B
VERTICAL SCALE	DESIGNATION
1:100	9328775
SURVEY BOOK	SHEETS
CONTRACT	6 of 27
B-22831	PROJECT
	BHF/097-7(009)

JTB 11/26/12
JEC 11/26/12



TYPICAL SECTION
Scale: 1/4" = 1'-0"

File Name: P:\C01\01-703\Bridges\Design\Preliminary Design\SR 37 over Stoney Creek\Typical Section.dwg Plot Date: 11/26/2012 Plotted By: JTB, Andrew J.



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RECOMMENDED FOR APPROVAL	
DESIGN ENGINEER	DATE
DESIGNED:	DRAWN:
CHECKED:	CHECKED:

INDIANA
DEPARTMENT OF TRANSPORTATION

SR 37
OVER STONEY CREEK

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION
SURVEY BOOK	SHEETS
CONTRACT	PROJECT

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202-51328

PRESENT STRUCTURE, REMOVE PORTIONS LS

SLAB $(196.0')(6.5' + 8.58')(\$8.00/\text{SFT}) = \$23,646$

RAILING $(196.0')(4)(\$12.00/\text{LFT}) = \$9,408$

WINGWALLS $(8.0' \times 14.76')(\$8.00/\text{SFT}) \times 4 = \$3,779$

DRAINS $6 \text{ EA} \times \$300/\text{EA} = \$1,800$

TOTAL $= \$38,633$

USE $\$40,000$

1 LS

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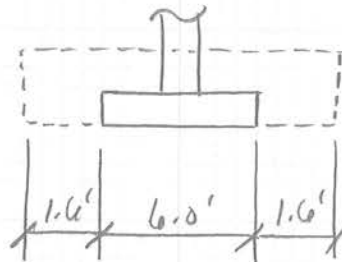
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206-51220

EXCAVATION, WET

CYS



AT 2 PLACES
PIERS #2 & #3
SOUTHBOUND = $(4.2' \times 9.0' \times 22.63') / 27 \times 2 = \underline{43.4 \text{ CYS}}$

AT 2 PLACES
PIERS #2 & #3
NORTHBOUND = $(4.2' \times 9.0' \times 19.75') / 27 \times 2 = \underline{55.3 \text{ CYS}}$

TOTAL
VOLUME = 118.7 CYS

118.7 CYS

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302 - 07455

DENSE GRADED SUBBASE

CYS

TOTAL AREA = 306.0 SYS (FROM APPROACH QNTY)
OF APPROACHES

DEPTH OF = 6" ≈ 0.167 yards
SUBBASE

TOTAL
VOLUME = 306.0 SYS x 0.167 yd = 51.10 CYS

51.10 CYS

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609 - 06257 REINFORCED CONCRETE BRIDGE APPROACH, 10 IN SYS

$$\text{WIDTH OF EXTENSION} = 17.0' /$$

$$\text{LENGTH OF OUTSIDE} = 43.6' @ \text{NORTH}, 20.5' @ \text{SOUTH}$$

$$\text{SKEW} = 30^\circ$$

$$\text{LENGTH OF OPP. SIDE} = 43.6' + (17.0' \times \tan 30^\circ) = 53.4' @ \text{NORTH}$$
$$20.5' @ \text{SOUTH}$$

$$\text{APPROACH AREA AT SOUTHBOUND} = \left[\frac{(43.6' + 53.4')}{2} \times 17.0' \right] / 9 +$$
$$[20.5' \times 17.0'] / 9 = 130.4 \text{ SYS}$$

$$\text{WIDTH OF EXTENSION} = 14.5' /$$

$$\text{LENGTH OF OUTSIDE} = 20.5' @ \text{SOUTH}, 45.0' @ \text{NORTH}$$

$$\text{SKEW} = 30^\circ$$

$$\text{LENGTH OF OPP. SIDE} = 45.0' + (14.5' \times \tan 30^\circ) = 53.4' @ \text{NORTH}$$
$$20.5' @ \text{SOUTH}$$

$$\text{APPROACH AREA AT NORTHBOUND} = \left[\frac{(53.4' + 45.0')}{2} \times 14.5' \right] / 9 +$$
$$[20.5' \times 17.0'] / 9 = 118.0 \text{ SYS}$$

$$\text{TOTAL APPROACH AREA} = 130.4 + 118.0 = 306 \text{ SYS}$$

306 SYS



UNITED
Consulting

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EACH

$$\frac{\text{TOTAL \# OF PILE TIPS}}{= 12 \text{ PILES} \approx \underline{\underline{12 \text{ TIPS}}}}$$

12 EA.

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701 - 95782 PILE, STEEL H, HP 12x84 LFT

OF PILES AT
4 PLACES = 3 piles x 4 bents = 12 PILES

PILE LENGTH = 15 FT

TOTAL LENGTH = 12 Piles x 15' = 180 LFT

180 LFT

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SHEET NO. _____ OF _____
JOB NO. 10-703

702 - 51005

CONCRETE A, SUBSTRUCTURE

CYS

PIER WALL
EXTENSION @ 1 PLACE

Pier #2 - S.B.

$$= \left[\frac{\underset{\text{WIDTH}}{19.63'} \times \underset{\text{HEIGHT}}{16.25'} \times \underset{\text{THICKNESS}}{2.0'}}{27} \right] \times 1$$
$$= \underline{23.6 \text{ CYS}}$$

PIER WALL
EXTENSION @ 1 PLACE

Pier #2 - N.B.

$$= \left[\frac{\underset{W}{16.75'} \times \underset{H}{16.25'} \times \underset{T}{2.0'}}{27} \right] \times 1$$
$$= \underline{20.2 \text{ CYS}}$$

PIER WALL
EXTENSION @ 1 PLACE
PIER #3 @ S.B.

$$= \left[\frac{\underset{W}{19.63'} \times \underset{H}{17.25'} \times \underset{T}{2.0'}}{27} \right] \times 1$$
$$= \underline{25.1 \text{ CYS}}$$

PIER WALL
EXTENSION @ 1 PLACE
PIER #3 @ N.B.

$$= \left[\frac{\underset{W}{16.75'} \times \underset{H}{17.25'} \times \underset{T}{2.0'}}{27} \right] \times 1$$
$$= \underline{21.4 \text{ CYS}}$$

TOTAL
VOLUME

$$= 23.6 + 20.2 + 25.1 + 21.4$$
$$= \underline{\underline{90.3 \text{ CYS}}}$$

90.3 CYS

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702-51015

CONCRETE B FOOTINGS

CYS

CONCRETE FOOTING
AT 2 PLACES = $(2.0' \times 6.0' \times 19.43') / 27 \times 2 = 17.5 \text{ CYS}$

PIER #2 & #3 - S.B.

CONCRETE FOOTING
AT 2 PLACES = $(2.0' \times 6.0' \times 16.75') / 27 \times 2 = 14.9 \text{ CYS}$

PIER #2 & #3 - N.B.

TOTAL = $17.5 + 14.9 = 32.4 \text{ CYS}$

32.4 CYS

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702 - 51110 GRATES, BASINS, AND FITTINGS CAST IRON LBS

DRAINS (TYPE SQ)

$$6 \text{ DRAINS @ } 215 \text{ LB} = \underline{1290 \text{ LBS}}$$

$$\underline{\text{TOTAL}} = \underline{1290 \text{ LBS}}$$

PIPES

$$\text{NO. OF DRAINS} = \underline{6}$$

$$\text{LENGTH PER PIPE} = \underline{4.083 \text{ ft}}$$

$$\text{TOTAL LENGTH OF PIPE} = \underline{24.5 \text{ ft}}$$

$$\text{WEIGHT OF PIPE} = \underline{18.1 \text{ LBS/ft}}$$

$$\underline{\text{TOTAL}} = \underline{444 \text{ LBS}}$$

$$\underline{\text{TOTAL WEIGHT}} = 1290 + 444 = \underline{\underline{1734 \text{ LBS}}}$$

1,734 LBS

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JOB NO. 10-703

702 - 92857

CONCRETE, C, SUBSTRUCTURE

CYS

BENTS BELOW
BEAM SEAT
@ 2 PLACES

(BENTS #2, #3 S.B.)

$$= \left(3.2' \times 3.0' \times 19.63' \right) / 27 \times 2 = \underline{21.3 \text{ cys}} \\ + (1.25' \times 4.0' \times 19.63')$$

BENTS BELOW
BEAM SEAT
@ 2 PLACES

(BENTS #2, #3 N.B.)

$$= \left(3.2' \times 3.0' \times 16.75' \right) / 27 \times 2 = \underline{18.1 \text{ cys}} \\ + (1.25' \times 4.0' \times 16.75')$$

WINGWALLS AT
4 CORNERS

MATCH EXIST.

$$= \left(8.0' \times 14.76' \times 1.0' \right) / 27 \times 4 = \underline{17.5 \text{ cys}}$$

$$\underline{\underline{\text{TOTAL VOLUME}}} = 21.3 + 18.1 + 17.5 = \underline{\underline{56.9 \text{ cys}}}$$

56.9 cys

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CHKD BY JEL DATE 11/26/12 JOB NO. 10-703

703 - 06028

REINFORCING BARS

LBS

$$\frac{\text{PIER WALL}}{\text{CONC. A}} = 90.3 \text{ CYS} \times 130 \#/\text{CYS} = \underline{11,739 \#}$$

$$\text{PIER FOOTING} = 32.4 \text{ CYS} \times 110 \#/\text{CYS} = \underline{3,564 \#}$$

$$\frac{\text{TOTAL}}{\text{LBS}} = \underline{\underline{15,303 \#}}$$

15,303 LBS

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703-06029

REINFORCING BARS EPOXY COATED

LBS

$$\text{SUPERSTRUCTURE} = 250.4 \text{ CYS} \times 250 \#/\text{CYS} = 62,600 \#$$

$$\text{SUBSTRUCTURE @ BENTS} = 56.9 \text{ CYS} \times 250 \#/\text{CYS} = 14,225 \#$$

$$\text{RAILING} = 392.0 \text{ FT} \times 26.3 \#/\text{FT} = 10,310 \#$$

$$\text{APPROACH SLAB} = 1306.05 \text{ YS} \times 35 \#/\text{YS} = 10,710 \#$$

$$\text{TOTAL WEIGHT} = \underline{\underline{97,845 \#}}$$

97,845 LBS

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CHKD BY SK DATE 11/26/12 JOB NO. 10-703

704-51602

CONCRETE C SUPERSTRUCTURE

CYS

$$\text{WIDTH OF DECK WIDEN} = 23.5 + 23.08' = \underline{46.58 \text{ FT}}$$

$$\text{LENGTH OTD} = \underline{196.0 \text{ FT}}$$

$$\text{THICKNESS} = \underline{0.67 \text{ FT}}$$

$$\text{VOLUME MAIN DECK WIDEN} = (46.58' \times 196.0' \times 0.67') / 27 = \underline{226.6 \text{ CYS}}$$

$$\text{BEAM FILLETS} = (0.167' \times 0.9583' \times 7 \times 172') / 27 = \underline{8.0 \text{ CYS}}$$

2" x 11 1/2" ON 7 BEAMS

$$\text{BENTS ABOVE BEAM SEAT @ 2 PLACES} = (3.33' \times 1.75' \times 19.63') / 27 \times 2 = \underline{8.5 \text{ CYS}}$$

BENTS #1, #4 @ S.B.

$$\text{BENTS ABOVE BEAM SEAT @ 2 PLACES} = (3.33' \times 1.75' \times 16.75') / 27 \times 2 = \underline{7.3 \text{ CYS}}$$

BENTS #1, #4 @ N.B.

$$\text{TOTAL VOLUME} = 226.6 + 8.0 + 8.5 + 7.3 = \underline{250.4 \text{ CYS}}$$

250.4 CYS

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CHKD BY SKL DATE 11/26/12 JOB NO. 10-703

706-09960

RAILING, CONCRETE, FC

LFT

$$\begin{array}{r} \text{BRIDGE LENGTH} = 196'-0'' \\ \times 2 \\ \hline \end{array}$$

$$\begin{array}{r} \text{TOTAL RAIL} \\ \text{LENGTH} = 392.0 \text{ FT} \end{array}$$

392.0 LFT

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CHKD BY JSC DATE 11/26/12 JOB NO. 10-703

709-51821

SURFACE SEAL

LS

$$\frac{\text{DECK WIDENING}}{\text{WIDTH}} = 23.5' + 23.08' = \underline{46.58 \text{ FT}}$$

$$\frac{\text{LENGTH}}{\text{LENGTH}} = \underline{196.0 \text{ FT}}$$

$$\frac{\text{AREA ON DECK}}{\text{AREA ON DECK}} = 46.58' \times 196.0' = \underline{9,129.7 \text{ FT}^2}$$

$$\frac{\text{AREA BOTTOM OF OVERHANG}}{\text{AREA BOTTOM OF OVERHANG}} = 4.12' \times 196.0' = \underline{807.5 \text{ FT}^2}$$

2.25' NORTH, 1.87' SOUTH

$$\frac{\text{AREA SIDE OF DECK}}{\text{8" DECK}} = 0.67' \times 196.0' \times 2 = \underline{262.6 \text{ FT}^2}$$

$$\frac{\text{AREA ON RAIL PERIMETER}}{\text{Perimeter} = 6.39 \text{ FT}} = 6.39' \times 2 \times 196.0' = \underline{2,504.9 \text{ FT}^2}$$

$$\frac{\text{TOTAL AREA}}{\text{TOTAL AREA}} = \underline{12,704.7 \text{ FT}^2}$$

#1 PER SFT OF SURFACE SEAL

$$1 \text{ LS} = \underline{\underline{\$12,705}}$$

1 LS

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711-51038

STRUCTURAL STEEL

LS

* 5 NEW BEAM LINES, 2 BMS N.B, 3 BMS S.B.
MATCH EXISTING BEAM SIZE \approx W33X130

$$\begin{aligned} \text{TOTAL BEAM LENGTHS} &= 193.0 \text{ FT} \quad (\text{MATCH EXIST. BM. LENGTHS}) \\ &\times 5 \\ &= \underline{\underline{965.0 \text{ FT}}} \end{aligned}$$

$$\text{BEAM WEIGHT} = \underline{\underline{130 \text{ LBS/FT}}}$$

$$\text{TOTAL BEAM WEIGHTS} = 965.0' \times 130 \text{ LBS/FT} = \underline{\underline{125,450 \text{ LBS}}}$$

$$* \text{ ADD } 15\% \text{ FOR MISC. STEEL} \approx \underline{\underline{144,268 \text{ LBS}}}$$

* ESTIMATE STEEL COST @ \$1.55/LB

$$\therefore \text{TOTAL COST} = 144,268 \times \$1.55 = \underline{\underline{\$223,616}}$$

1 LS

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BY JTB DATE 11/26/12 SUBJECT SR37 MOBILITY SHEET NO. _____ OF _____
CHKD BY JEC DATE 11/26/12 JOB NO. 10-703

724-51925

STRUCTURAL EXPANSION JOINT, SS

LFT

LENGTH OF JOINT
AT EA. END OF = 65.8 FT
BRIDGE X 4 ENDS
TOTAL = 263.2 LFT
LENGTH

263.2 LFT

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BY JTB DATE 11/26/12 SUBJECT SR37 MOBILITY SHEET NO. _____ OF _____
CHKD BY JE DATE 11/26/12 JOB NO. 10-703

726-11448 BEARING ASSEMBLY, EXPANSION, TYPE 1 EA.

AT EA. BEAM
AND BENT #1 & #4 = 5 BMS x 3 = 15 EA.
PIER #2

15 EA.

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BY JTB DATE 11/26/12 SUBJECT SR37 MOBILITY SHEET NO. _____ OF _____
CHKD BY JEC DATE 11/26/12 JOB NO. 10-703

726-11451 BEARING ASSEMBLY, FIXED, TYPE 1 EA.

AT EA. BEAM = 5 BMS X 1 = 5 EA.
AND PIER #3

5 EA.

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BY JTB DATE 11/26/12 SUBJECT SR37 MOBILITY SHEET NO. _____ OF _____
CHKD BY Sec DATE 11/26/12 JOB NO. 10-703

801 - 06203 TEMPORARY PAVEMENT MARKING, 4IN. LFT

BARRIER LENGTH = 466.5 FT FA. SIDE (SEE TEMP. TRAFFIC BARRIER)

ADDITIONAL LENGTH = 100.0'

FLARE LENGTH = 200.0'
20' OFFSET
AT 10:1

TOTAL LENGTH = 766.5' x 2 sides = 1533 LFT

1533 LFT

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BY JTB DATE 11/26/12 SUBJECT SR37 MOBILITY SHEET NO. _____ OF _____
CHKD BY SK DATE 11/26/12 JOB NO. 10-703

801-06710

FLASHING ARROW SIGN

DAY

6 Mo. x 30 DAYS = 240 DAYS

x 2 LANES

480 DAYS

480 DAYS

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801 - 06775

MAINTAINING TRAFFIC

LS

SAY \$20,000

1 LS

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BY JTB DATE 11/26/12 SUBJECT SR37 MOBILITY SHEET NO. _____ OF _____
CHKD BY JFK DATE 11/26/12 JOB NO. 10-703

801-08508 TEMPORARY TRAFFIC BARRIER, TYPE 2, ANCHORED LFT

BRIDGE LENGTH = 196.0 FT

APPROACH LENGTH = 20.5 FT

ADDITIONAL LENGTH = 50.0 FT

FLARE LENGTH = 200.0 FT

20' OFFSET
AT 10:1

TOTAL LENGTH = $(196' + 20.5' + 50.0' + 200.0') \times 2$
= 933.0 LFT

933 LFT